

# STREETSCAPE DESIGN FRAMEWORK

## INTRODUCTION

The hierarchy of the current vehicular circulation system within the Town Center provides the opportunity to create a logical and identifiable series of streetscapes. As has been discussed, these streetscapes were identified in the internal development of the Town Center Analysis Areas. The Streetscape Framework has been designed not only to enhance the experience of moving to and through the Town Center by automobile, bicycle, and on foot, but to reinforce orientation, identity and a sense of place.

The major highways of MD-118 and Father Hurley Boulevard constitute the primary roads within the Town Center. MD-118 serves as the primary entrance to the Town Center. The streetscape of MD-118 emphasizes its significance with a formal arrangement of street trees, flanking the sidewalks on both sides of the roadway, and with a single row in the median. The Town Center **gateways** at I-270 and the CSX Railroad are emphasized with double rows of ornamental trees.

Father Hurley Boulevard traverses and links residential areas and, as such, has a different character than MD-118. The Father Hurley Boulevard streetscape is recommended as less formal and more natural in appearance. Setbacks to buildings are also greater, with the intent of enhancing the open space between the highway and adjacent development.

Second in the overall Town Center streetscape hierarchy are the major highways of Middlebrook Road, Crystal Rock Drive, and the major arterial of Wisteria Drive. These roadways provide access to the Town Center, connect the major through roads of MD-118 and Father Hurley Boulevard and provide access to development within the Town Center Analysis Areas of TC-1, TC-2, TC-5, and the existing shopping centers and related development.

The character of these streetscapes, although formal, is recommended to be less monumental than those of the major through roadways. The existing sidewalks will remain five feet from the curb, with single rows of street trees planted beyond. Single rows of trees will also be planted in the medians of Middlebrook Road and Crystal Rock Drive.

Third in the hierarchy are the secondary roads of Locbury Drive and Waters Road, Old MD-118, and Walter Johnson Drive. These streets carry local vehicular and pedestrian traffic between zones of the Town Center. They also serve to connect the major open space systems. Their streetscapes are designed to enhance pedestrian and bicycle movement. Locbury Drive is proposed to incorporate a park-like character with lawn areas and informal plantings extending beyond the sidewalk and the hiker/biker trails.

Old MD-118 will incorporate street trees, lighting, and other street furniture that extend the historic character of the recently reconstructed MARC Commuter Rail Station and the Historic District.

Fourth in the streetscape hierarchy are those streets internal to the Analysis Areas of the Town Center. Within TC-1 the major street of Town Center Boulevard is the MD-118 entrance to the Town Center Core. This street is anticipated to carry the highest volume of traffic to TC-1 and is recommended to have a wide section. As the main entrance it has a more monumental streetscape than the other streets. The boulevard will have a 28- to 30-foot median, planted with street trees, four travel lanes, and two parking lanes. The sidewalk will be primarily **hard edge**, to accommodate pedestrian use and to reinforce the commercial character. Street trees will be planted in cut-outs, tree grates, or planters within the pavement.

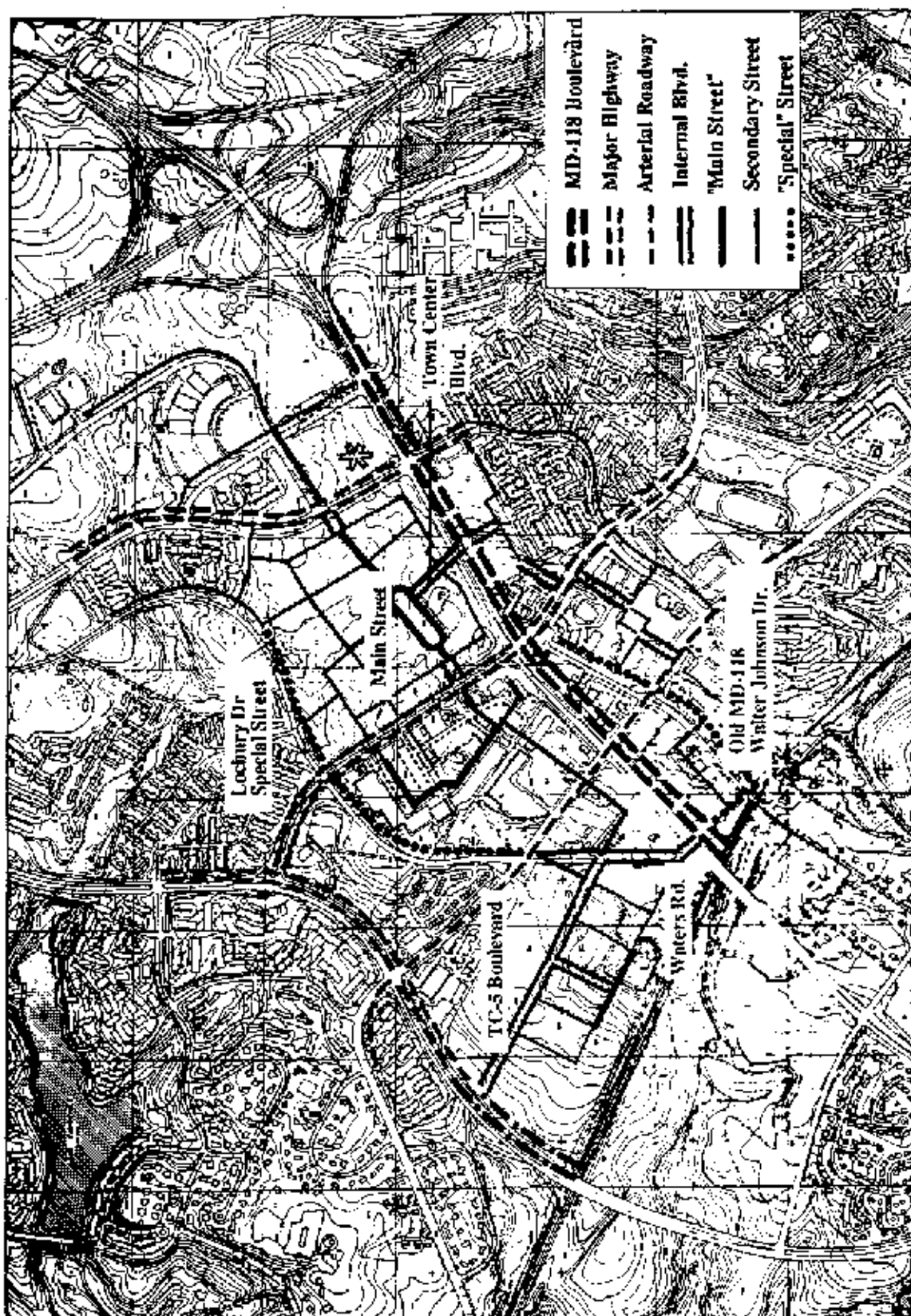
Main Street is another major street in the TC-1 hierarchy. Connecting Middlebrook Road and Crystal Rock Drive through the Core, Main Street is the downtown shopping street of the Town Center. To emphasize pedestrian usage, this street will have fewer travel lanes and a **hard edge**. To provide a more intimate cross-section buildings are to be closer to the curb than on the Boulevard. Where Main Street follows the perimeter of the Town Center Commons, the streetscape will be the same on both sides in order to provide the desired continuity. The exception is proposed in relation to the Cultural Arts Center where additional setbacks, sidewalk, and landscaping is recommended. Street trees will be planted in cut-outs, tree grates, or planters within the sidewalk.

Next in the hierarchy of TC-1 streetscapes are those associated with the secondary streets that provide access to the Core. These streets incorporate a **soft edge**, consisting of grassed planting strips between the curb and the sidewalk and street trees.

Last in the TC-1 streetscape hierarchy are those streets which provide access to and through parking areas. Street edges that abut commercial or office buildings will have a **hard edge**, with cut-outs and planting beds incorporated. Those adjacent to residences will have a **soft edge** consisting of grass strip or ground cover between the curb and the sidewalk planted with street trees. A planting area is to be provided between the sidewalk and the building as well.

The major streetscapes in the hierarchy internal to TC-5 are associated with those streets that provide access from MD-118, Father Hurley Boulevard, and Wisteria Drive. These streets are four lanes in width and contain a median. Trees will be planted both in the median and on both sides (between the curb and the sidewalk).

Next in the hierarchy is the axial boulevard leading from the high-rise residential tower to the middle of the retail shopping center. This boulevard has fewer lanes than the major boulevards and will have smaller ornamental trees in the median to signal arrival.



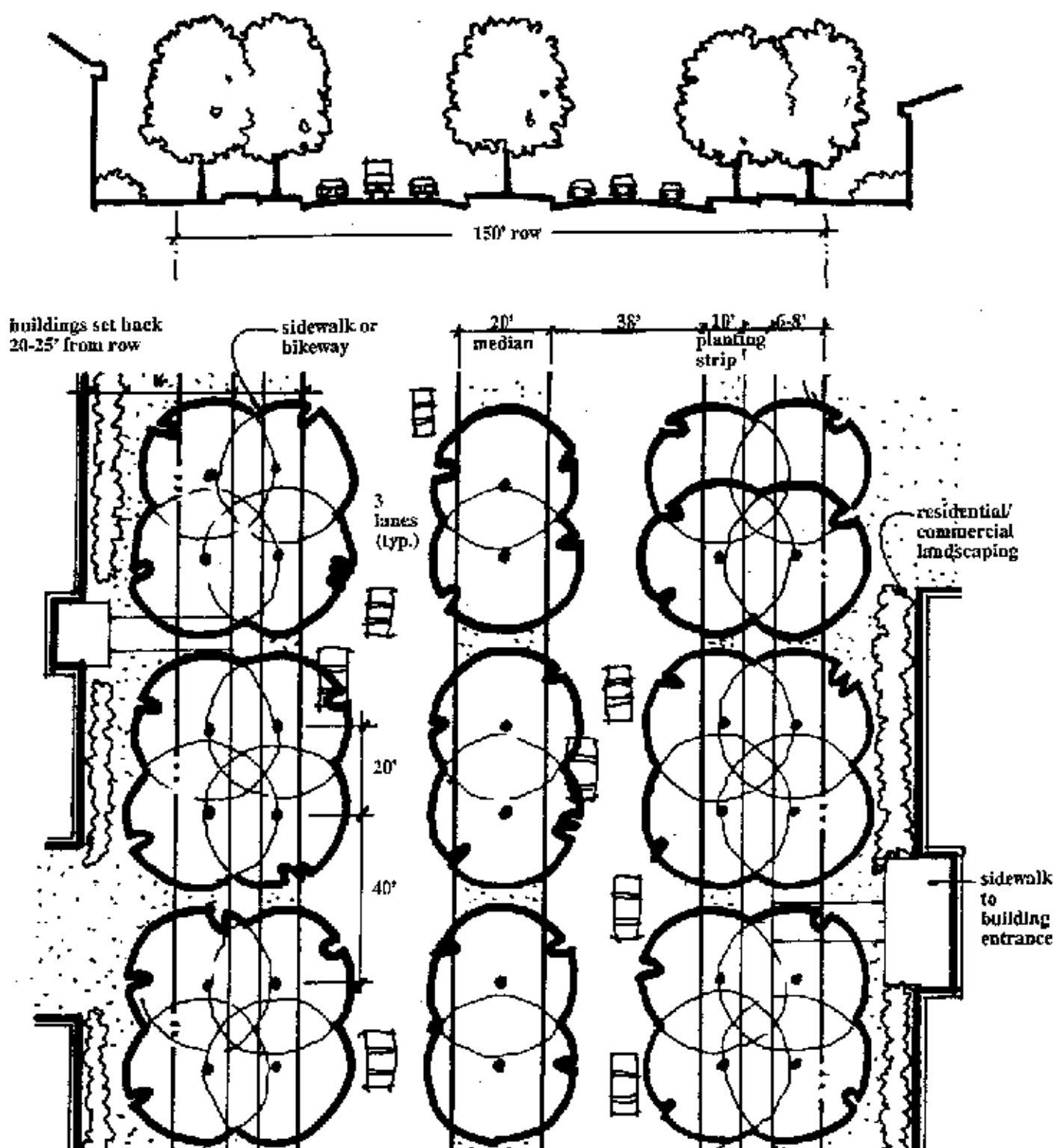
## DESIGN PROTOTYPES

### MARYLAND ROUTE 118

MD-118 is the primary access highway to the Germantown Town Center and also serves through traffic. MD-118 consists of a 150-foot right-of-way with six lanes and a 20-foot median. (Although master planned at 24 feet, these medians were constructed at 20 feet in width.) The proposed streetscape for MD-118 has a formal pattern of street trees, sidewalks, and bike paths.

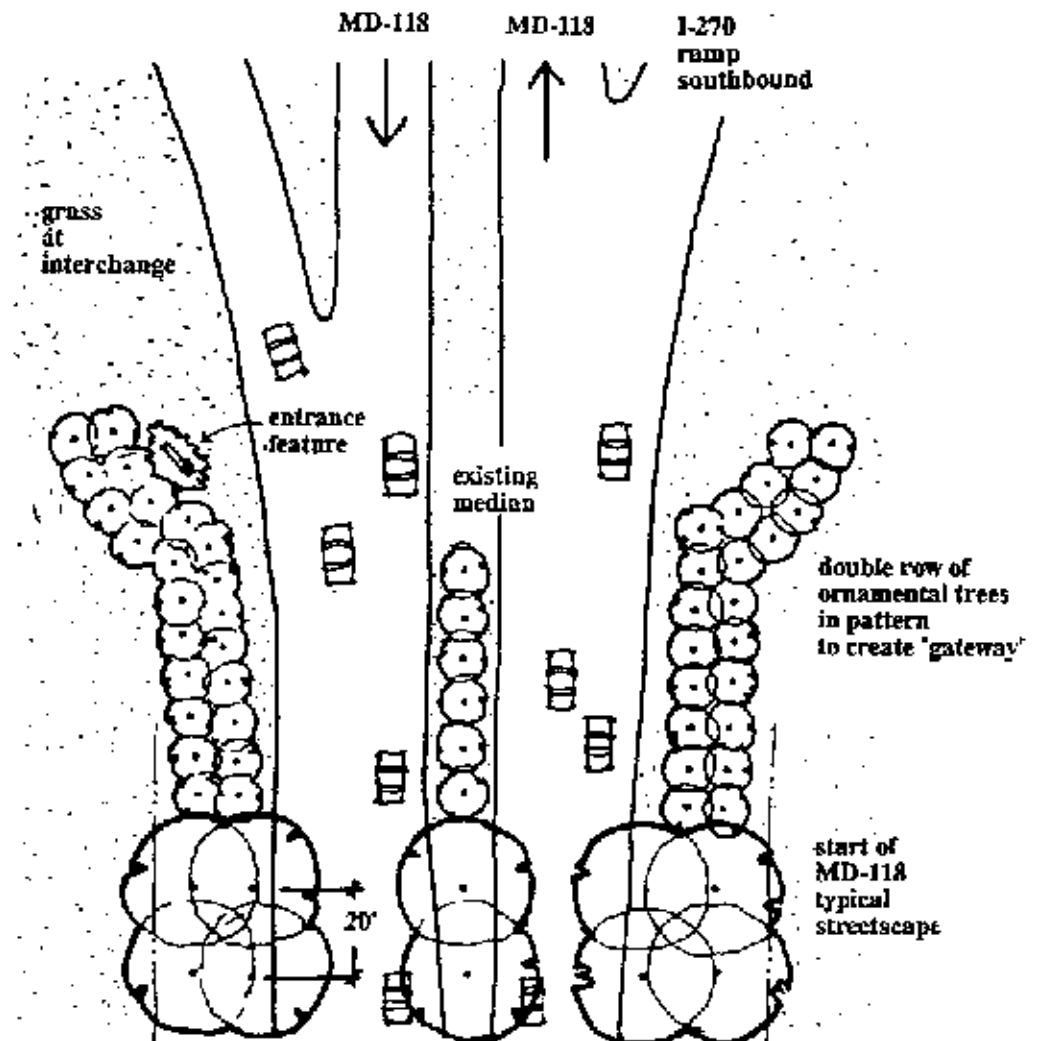
**TYPICAL** The typical streetscape along MD-118 will consist of a six-foot sidewalk or eight-foot bike path set back ten feet from the roadway. A double row of trees will flank the sidewalk and bike path in an alternating 20-foot/40-foot spacing pattern. This will result in regularly-spaced clusters of four trees. The intent of this pattern is to establish a formal green edge to the roadway. The 40-foot openings will allow views beyond the roadway to adjacent buildings and open space. Red Oak, a species that has a stately, formal form, is proposed. Buildings are recommended to be set back 20 to 25 feet from the street right-of-way. This is desired to allow sufficient area for expansion of the open space adjacent to the streetscape proposed within the right-of-way. It is, however, understood that not all buildings can or should be set back this distance and the flexibility is provided to allow some buildings to be located closer to the right-of-way pending site plan review.

The 20-foot median of MD-118 will also incorporate a single row of trees in the same 20-foot/40-foot pattern.

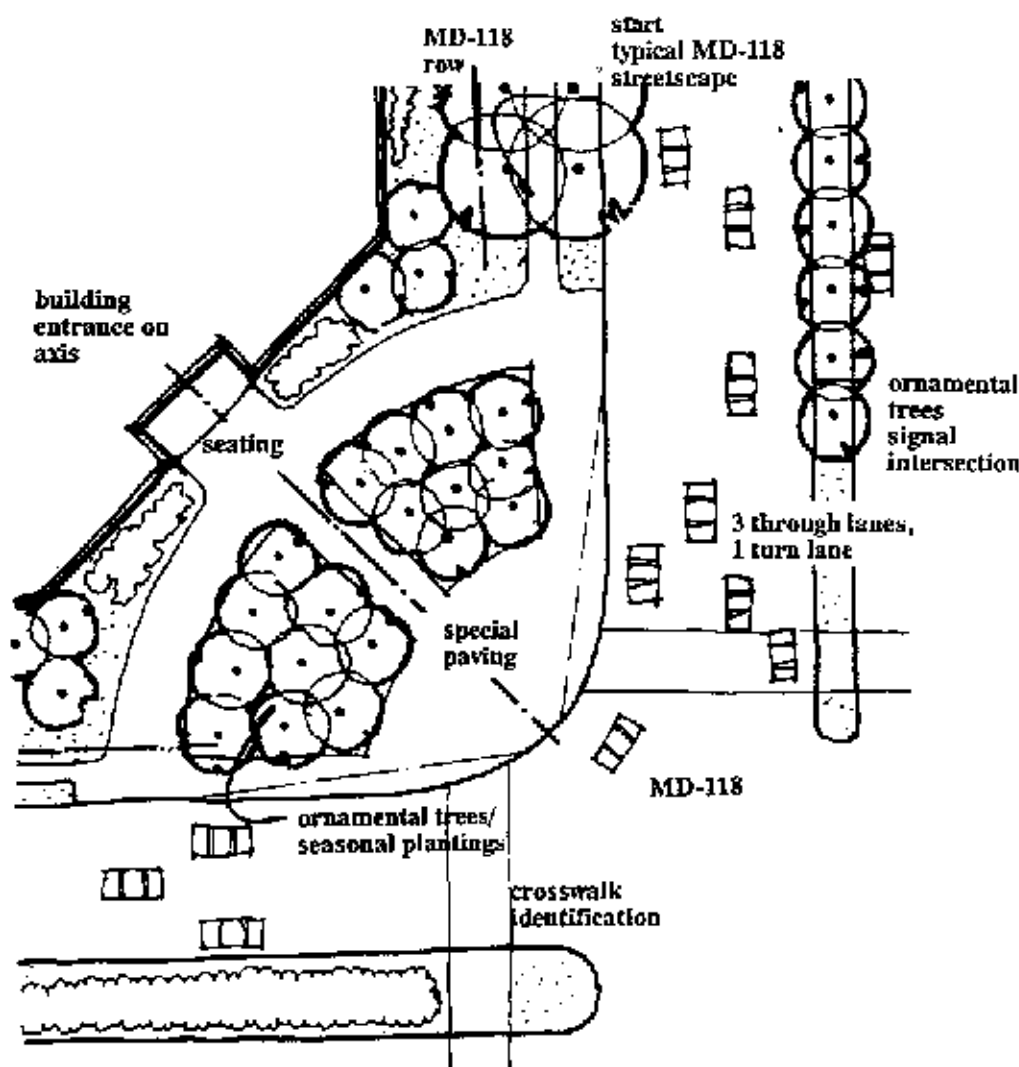


MD-118 BOULEVARD - TYPICAL CONDITION

**GATEWAYS** At the Town Center's major MD-118 gateway (from I-270), the character of the entry is established by formally arranged masses of ornamental trees. Double rows of ornamental trees are proposed to flank the roadway for a distance of 100 feet. A single row of ornamental trees are recommended to be planted in the median. The first row is to be set back ten feet from the curb. Although no sidewalk is currently planned for this portion of MD-118, because of the I-270 traffic, consideration should be given to the provision of pedestrian circulation over I-270. The gateway at MD-118 and the CSX Railroad tracks is proposed to incorporate a similar streetscape treatment.

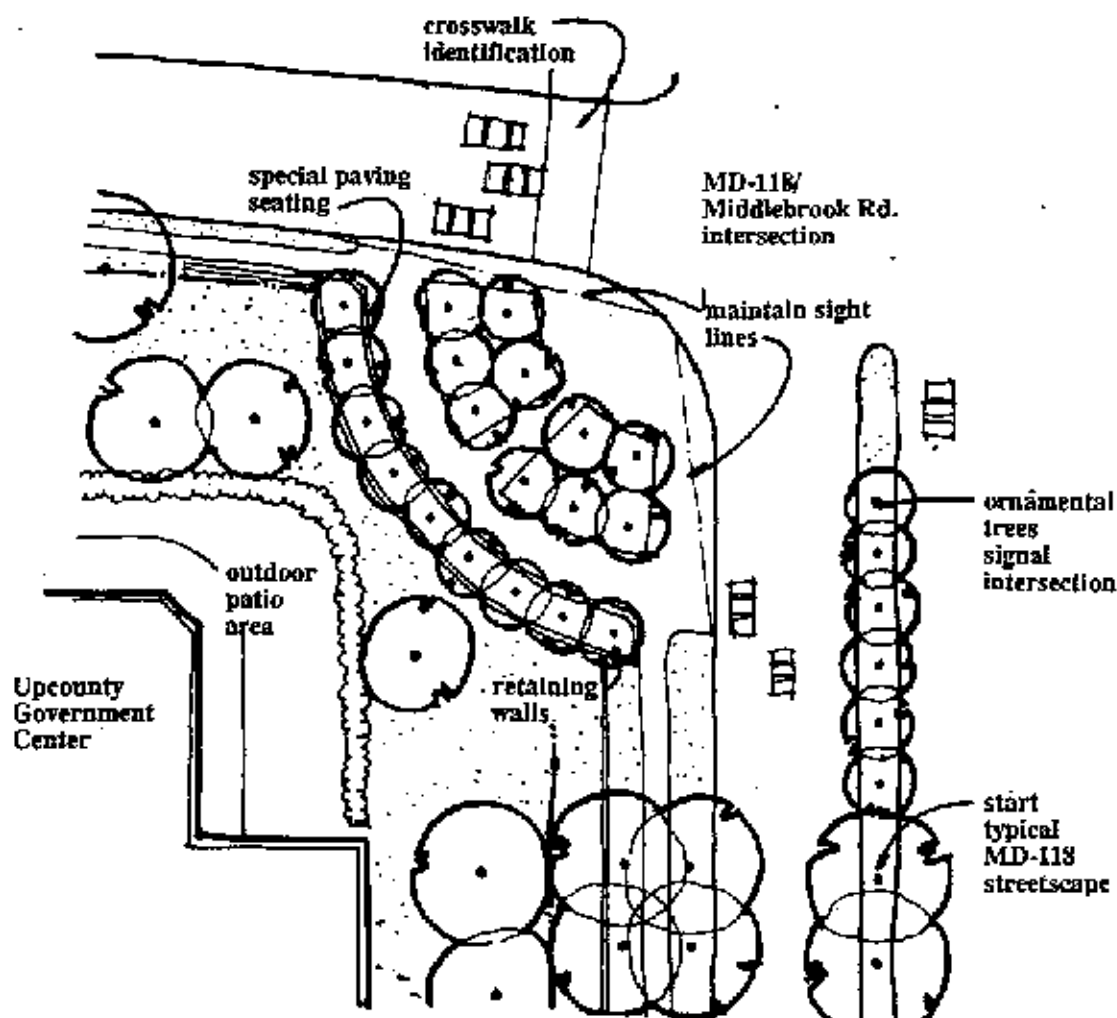


**INTERSECTIONS/CORNERS** At the major intersections of Crystal Rock Drive, Middlebrook Road and Wisteria Drive - trees, sidewalks, and pedestrian areas are recommended to be arranged to create the character of a pedestrian entry. A single row of ornamental trees in the median will signal arrival. Planting beds with double rows of ornamental trees are proposed to provide a colorful backdrop for the pedestrian areas to be incorporated on these corners. Benches, lighting, and other site furniture are recommended to encourage use of these areas.



TYPICAL CORNER (TC-1, TC-2)

Corners of major intersections are also designed to encourage pedestrian activity. Enlarged areas of pedestrian paving will be provided where sidewalks meet. Ornamental trees and associated plantings and garden areas are recommended to provide visual interest and a backdrop for pedestrian activity. Retaining walls may be incorporated in selected steeply sloped areas to accommodate streetscape plantings (i.e., the Upcounty Government Center).

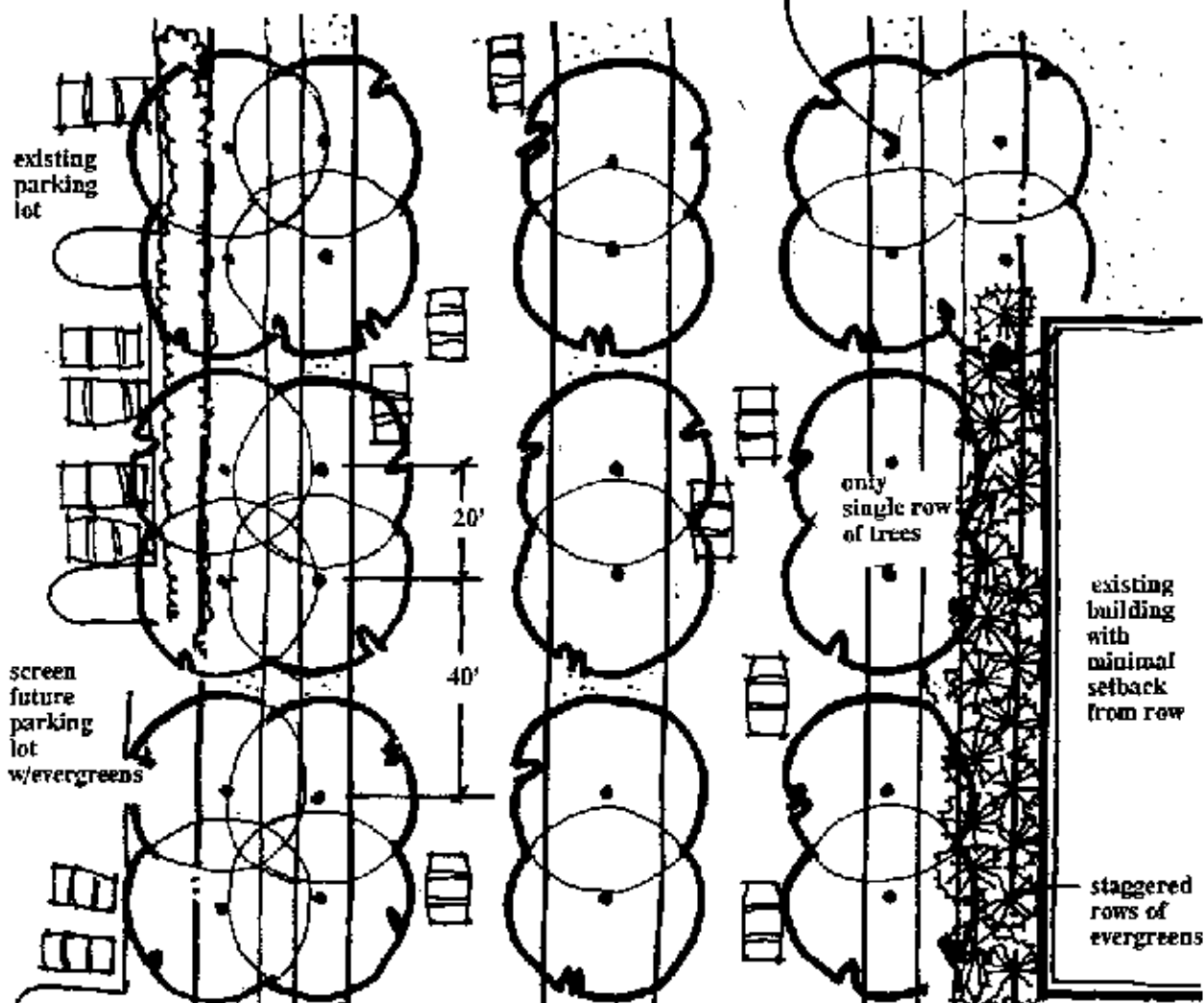


#### CORNER (UPCOUNTY GOVERNMENT CENTER)



**SCREENING** In locations where existing buildings or parking are adjacent to the roadway, the typical MD-118 streetscape may be required to be altered. Staggered rows of evergreen trees are proposed to be planted between the sidewalk and the buildings. Existing parking areas are proposed to incorporate low walls and/or be screened by hedges, shrubs and trees. Future parking lots are proposed to also include a double row of evergreen trees.

screen parking  
with hedge  
and/or wall



TYPICAL SCREENING

## MIDDLEBROOK ROAD AND CRYSTAL ROCK DRIVE

Middlebrook Road and Crystal Rock Drive both serve as major distributors connecting MD-118, Father Hurley Boulevard, etc., to the various sites within the Town Center. Although still formal in nature, their proposed streetscapes are recommended be less monumental than that of MD-118.

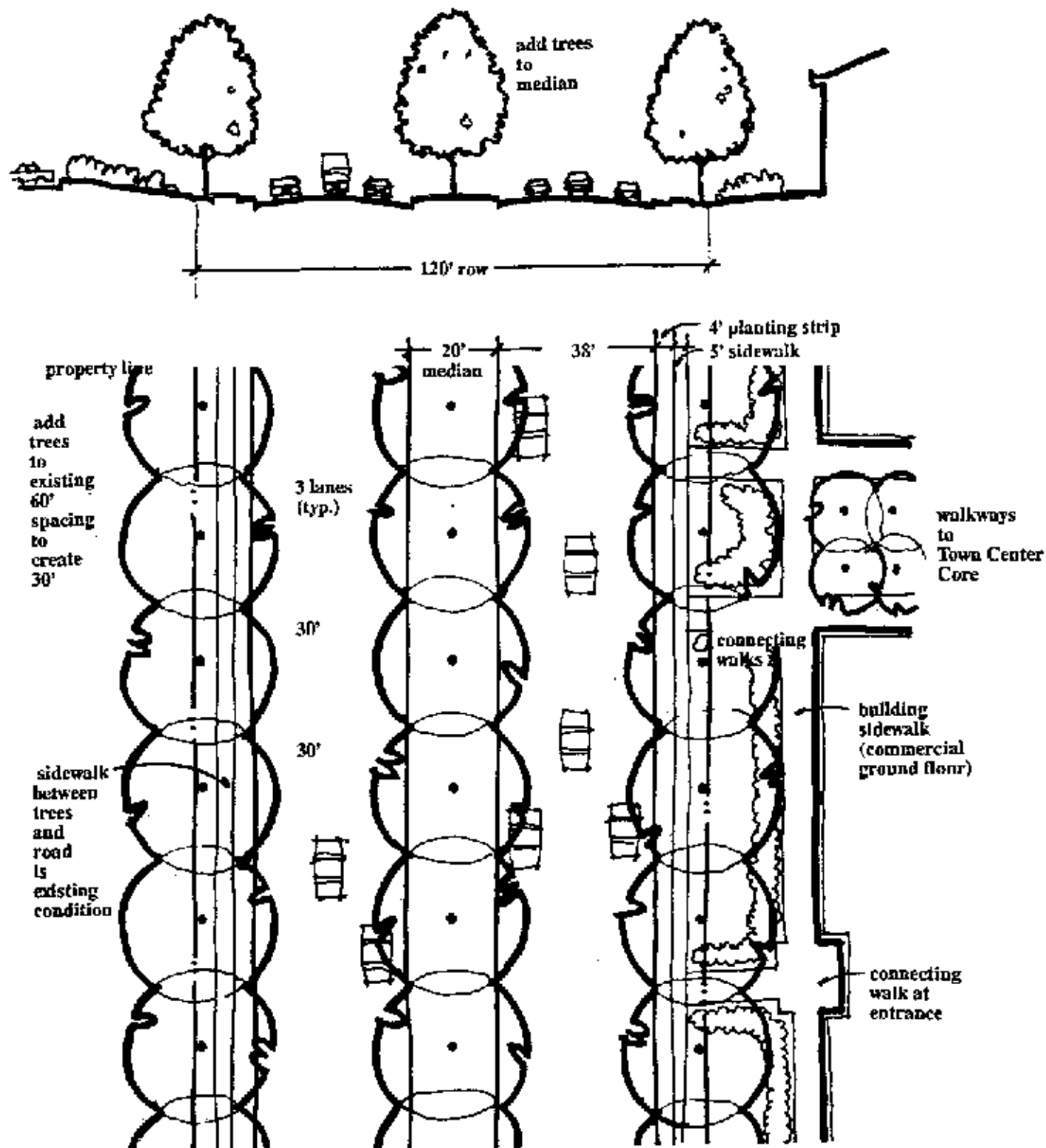
Both streets consist of a 120-foot right-of-way with four lanes and a median. The Germantown Master Plan recommends six lanes for both roads.

### **TYPICAL - MIDDLEBROOK ROAD**

The existing streetscape on Middlebrook Road between MD-118 and Father Hurley Boulevard typically consists of a five-foot sidewalk set back four feet from the roadway. An existing eight-foot bike path is set back two to twenty feet from the roadway on the west side, south of MD 118. A bike path is recommended on the east side of Middlebrook Road from MD-118 to Locbury Drive. Street trees (Willow Oaks) are planted approximately three feet beyond the sidewalk, 60 feet on center on both sides of the road. They are not planted in the median which is 24 feet in width. New Willow Oaks are proposed to be planted between the existing trees to establish a 30-foot spacing. The median is also recommended to be planted with Willow Oaks, 30 feet on center in areas where the future force main sewer is not proposed to be located. Where this force main sewer is proposed to be constructed in the median no trees can be accommodated. It is also recommended that consideration be given to locating this force main on the TC-1 side of Middlebrook Road between the trees and future development, where feasible, depending on edge conditions.

### **TYPICAL - CRYSTAL ROCK DRIVE**

The existing streetscape on Crystal Rock Drive between MD-118 and Father Hurley Boulevard consists of a five-foot sidewalk set back four feet from the roadway. An eight-foot bike path is proposed on the east side of the roadway. Street trees are proposed to be planted 30 feet on center, beyond the sidewalk on both sides of the roadway as well as in the median. A species of Zelkova is recommended. As on Middlebrook Road, buildings are proposed to be set back a minimum of 25 feet from the right-of-way. For retail or office use, a building sidewalk is also recommended.

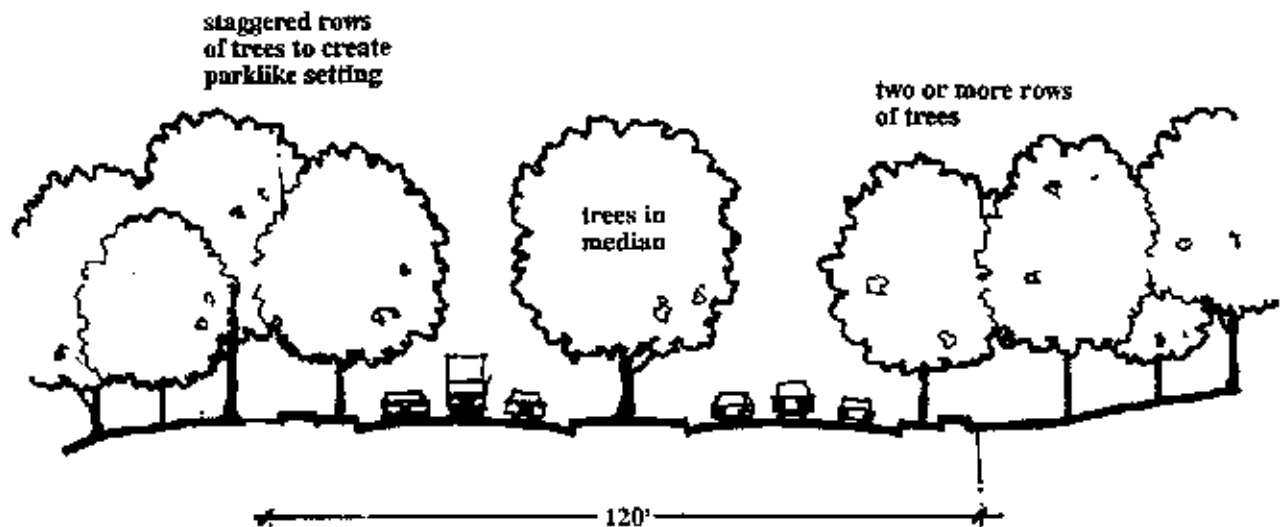


MIDDLEBROOK ROAD AND CRYSTAL ROCK DRIVE

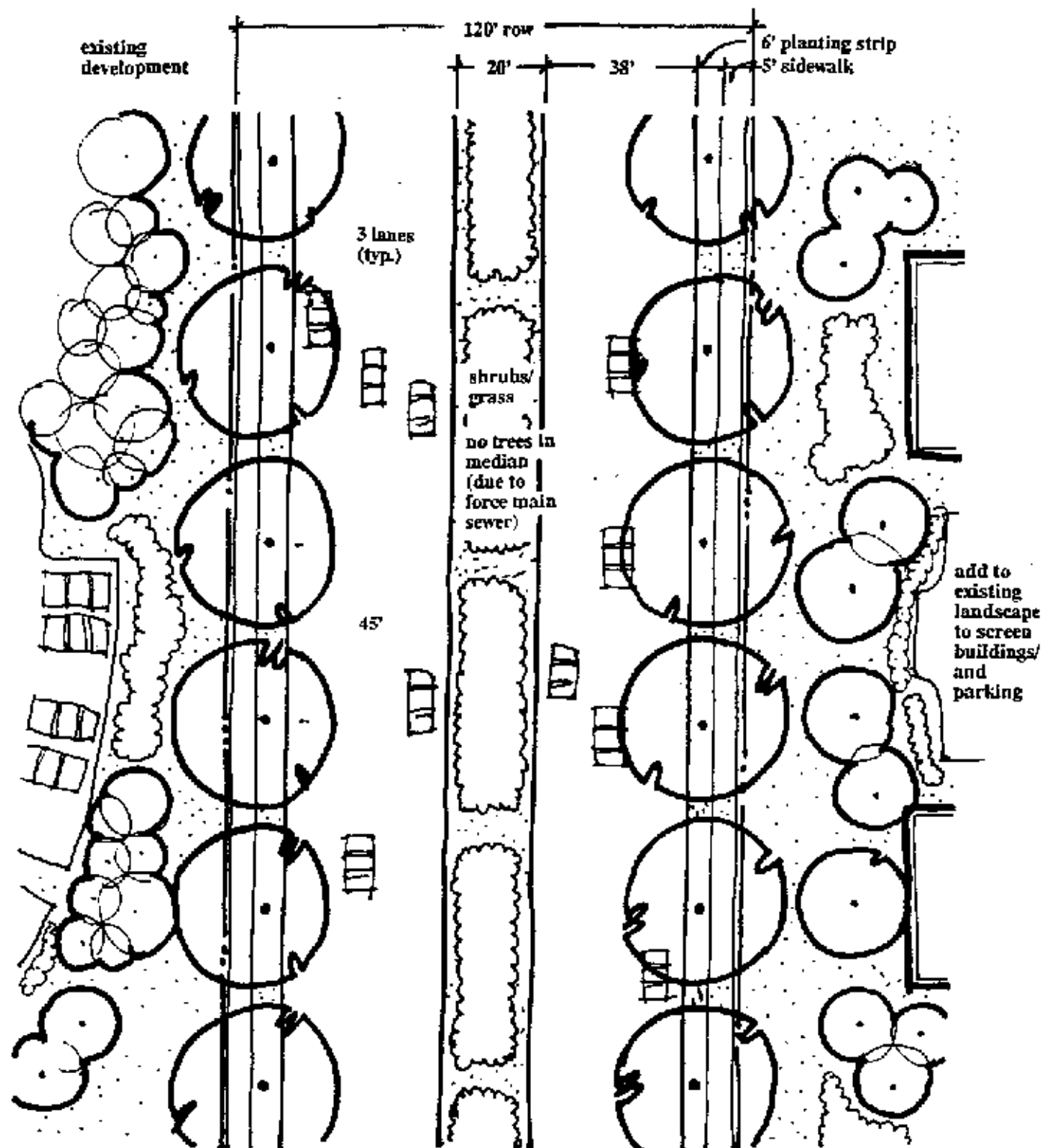
## FATHER HURLEY BOULEVARD

Father Hurley Boulevard is a major highway that forms the boundary between the Town Center and residential development to the west. It consists of a 120-foot right-of-way, currently with four lanes and a median. The master plan recommends that it have six lanes and a 24-foot median.

**TYPICAL - EXISTING DEVELOPMENT** The existing streetscape on Father Hurley Boulevard as constructed to Wisteria Drive, includes a five-foot sidewalk on one side set back six feet from the curb. Street trees (Red Oaks) are planted beyond the sidewalk, 45 feet on center and on both sides of the road. No trees are currently in the median. A sidewalk will be added to the south side of the road. A bike path will be added instead of a sidewalk on the south side of Father Hurley from the Town Center entrance to the TC-5 Boulevard and Father Hurley Park. The proposed force main sewer is proposed to be constructed in the median and no trees can be located here; shrubs are recommended. However, in the to-be-developed right-of-way below Wisteria Drive, this sewer is proposed to be located on the side of the roadway.



## FATHER HURLEY BOULEVARD - TOWN CENTER GATEWAY

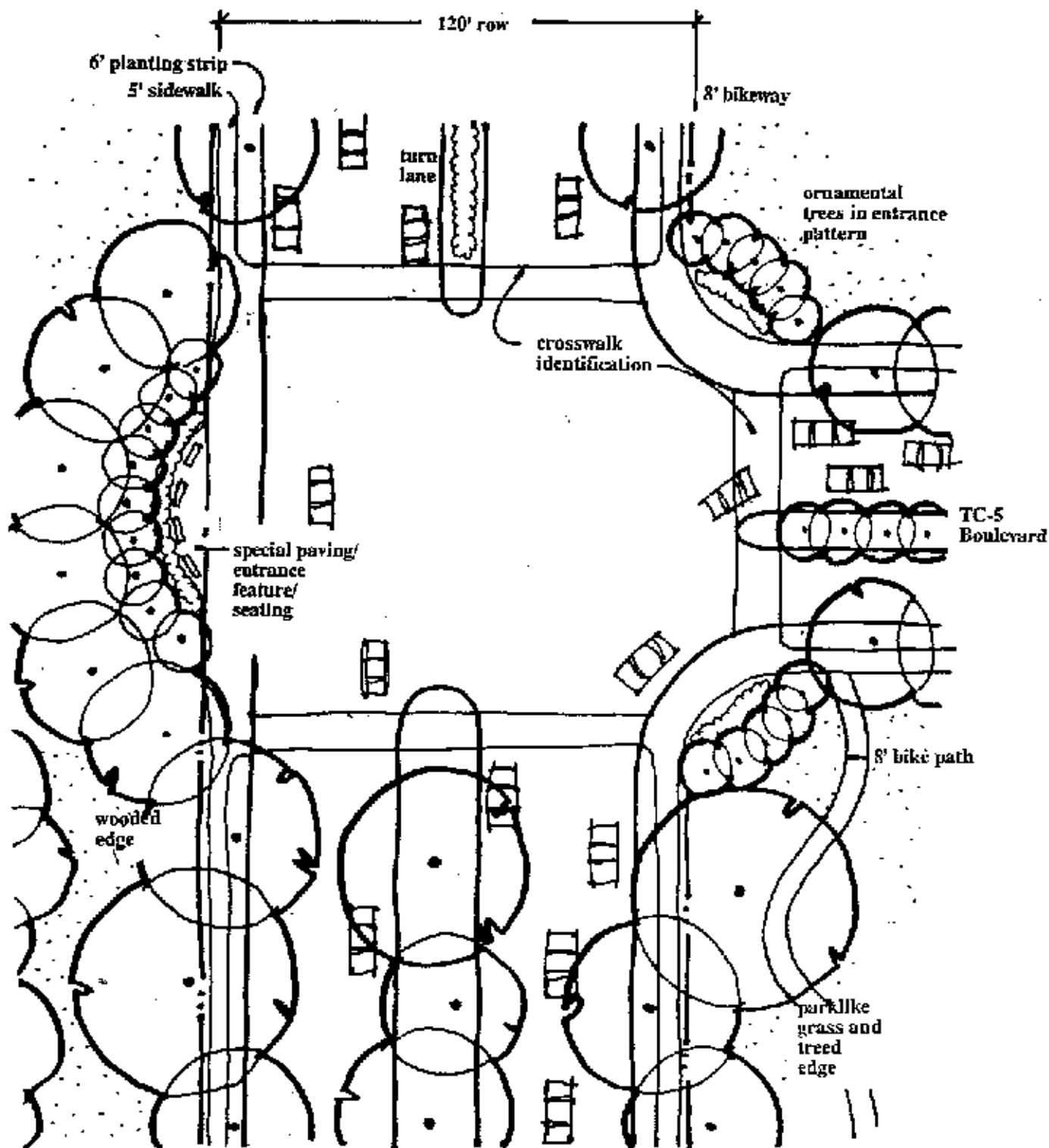


FATHER HURLEY BOULEVARD

**TOWN CENTER GATEWAY/TC-5 ENTRANCE**

The less formal, natural character of the area near the CSX Railroad tracks provides the opportunity for the creation of a park-like **gateway** to the Town Center. Larger, less formal tree species are to be incorporated at irregular spacings (both on the sides of the roadway and in the median) to strengthen the natural character of this entrance. This area is also part of a major **to be committed** open space system.

The transition between the typical Father Hurley Boulevard streetscape and the **gateway** streetscape will occur at the intersection of the TC-5 entrance boulevard. A single row of ornamental trees in the median, along with the plantings of ornamental trees at each corner and across Father Hurley Boulevard opposite the entrance, will signal the intersection. Pedestrian crosswalks and widened sidewalks, are proposed to identify and accommodate pedestrian activity at this intersection.

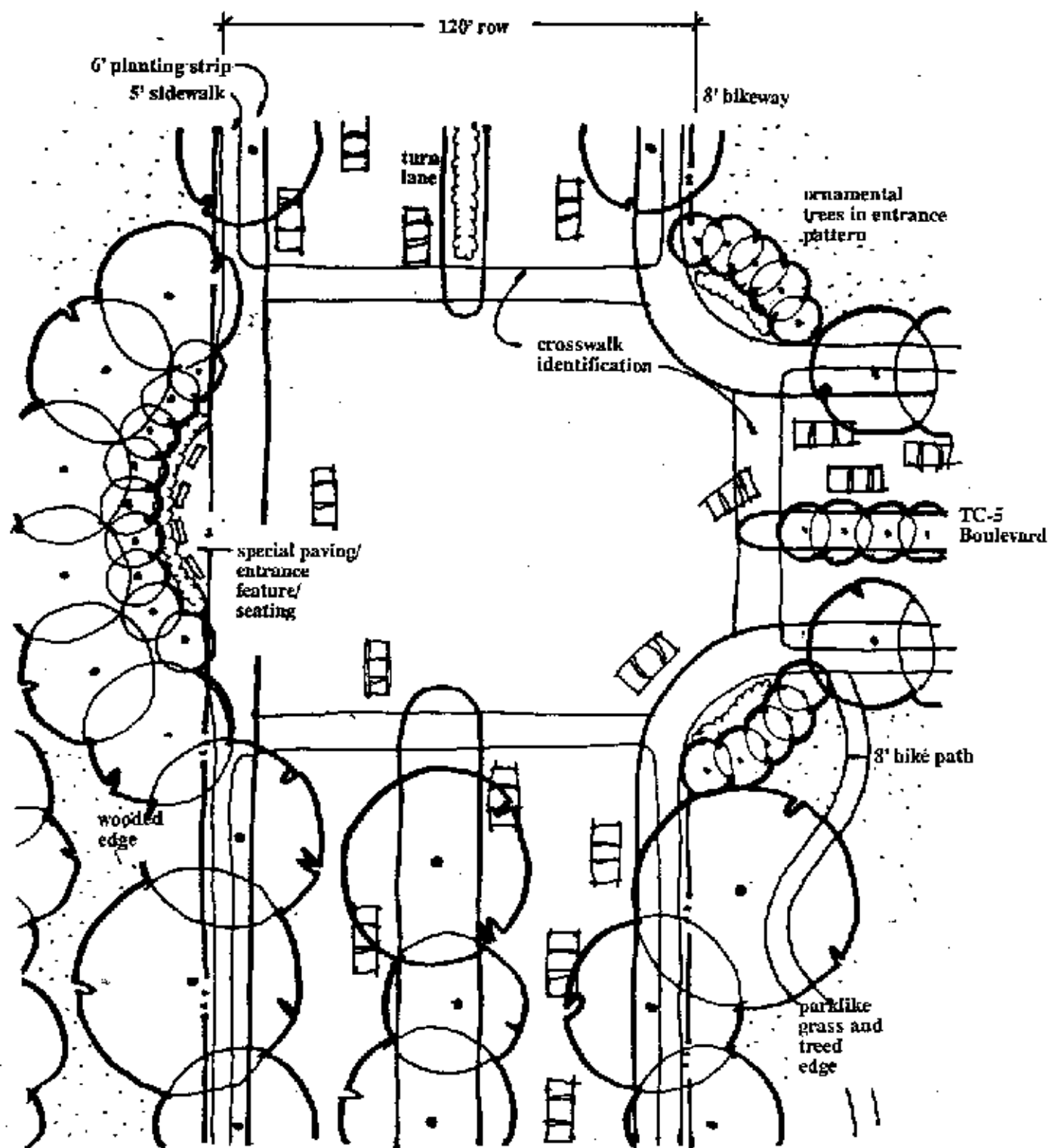


FATHER HURLEY BOULEVARD - TC-5 ENTRANCE BOULEVARD

## WISTERIA DRIVE

Wisteria Drive, from Father Hurley Boulevard to Great Seneca Highway, is a four-lane roadway with no median within an 80-foot right-of-way. Sidewalks, five feet wide, are proposed to be set back from the curb eight to ten feet on both sides. Currently, Sugar Maples are planted 60 feet on center on both sides of a majority of Wisteria Drive (between Father Hurley Boulevard and MD-118). New Sugar Maples are proposed to be planted to establish a 30-foot spacing. On the east side of MD-118, existing oaks will be supplemented to achieve a similar spacing.

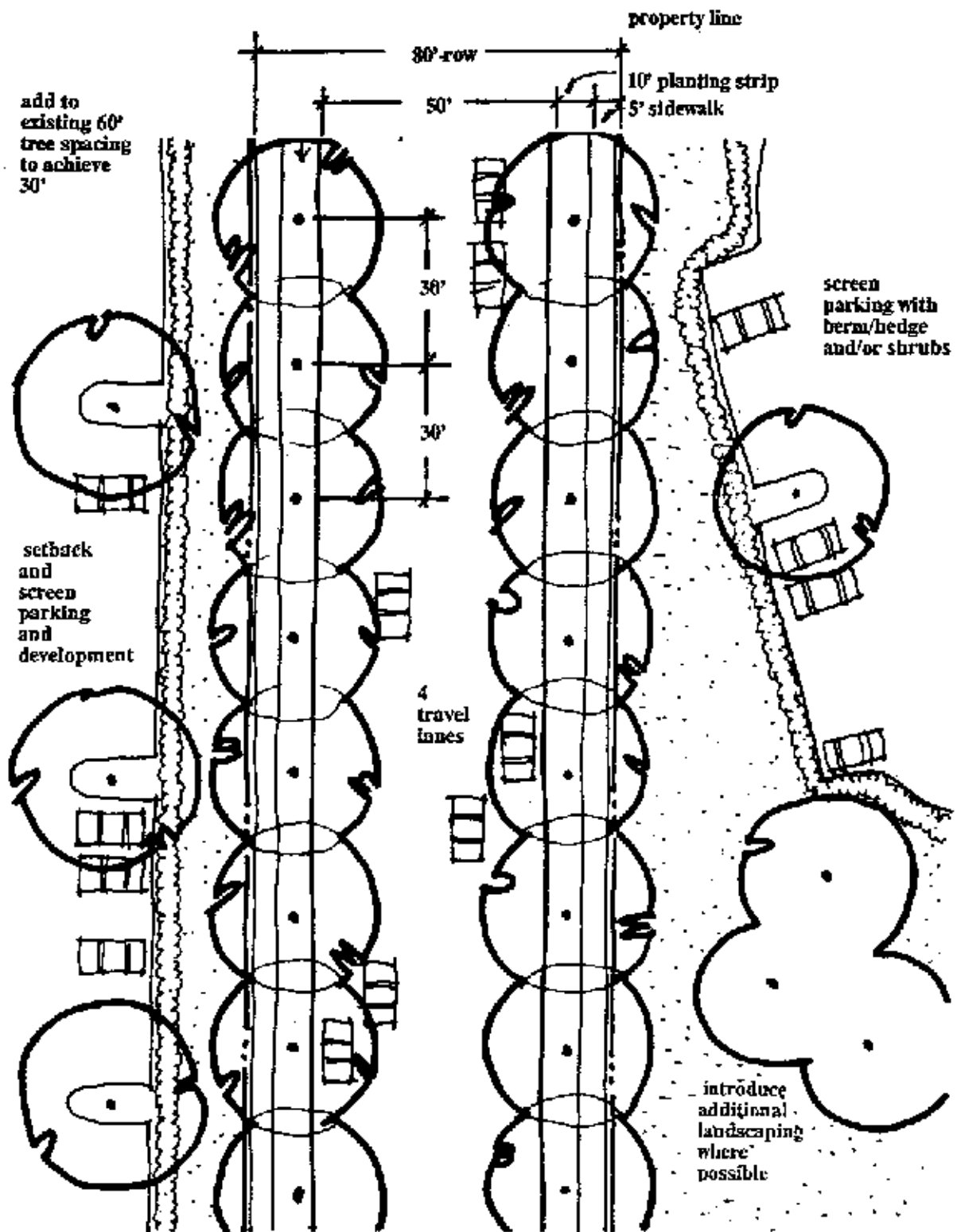




FATHER HURLEY BOULEVARD • TC-5 ENTRANCE BOULEVARD

## WISTERIA DRIVE

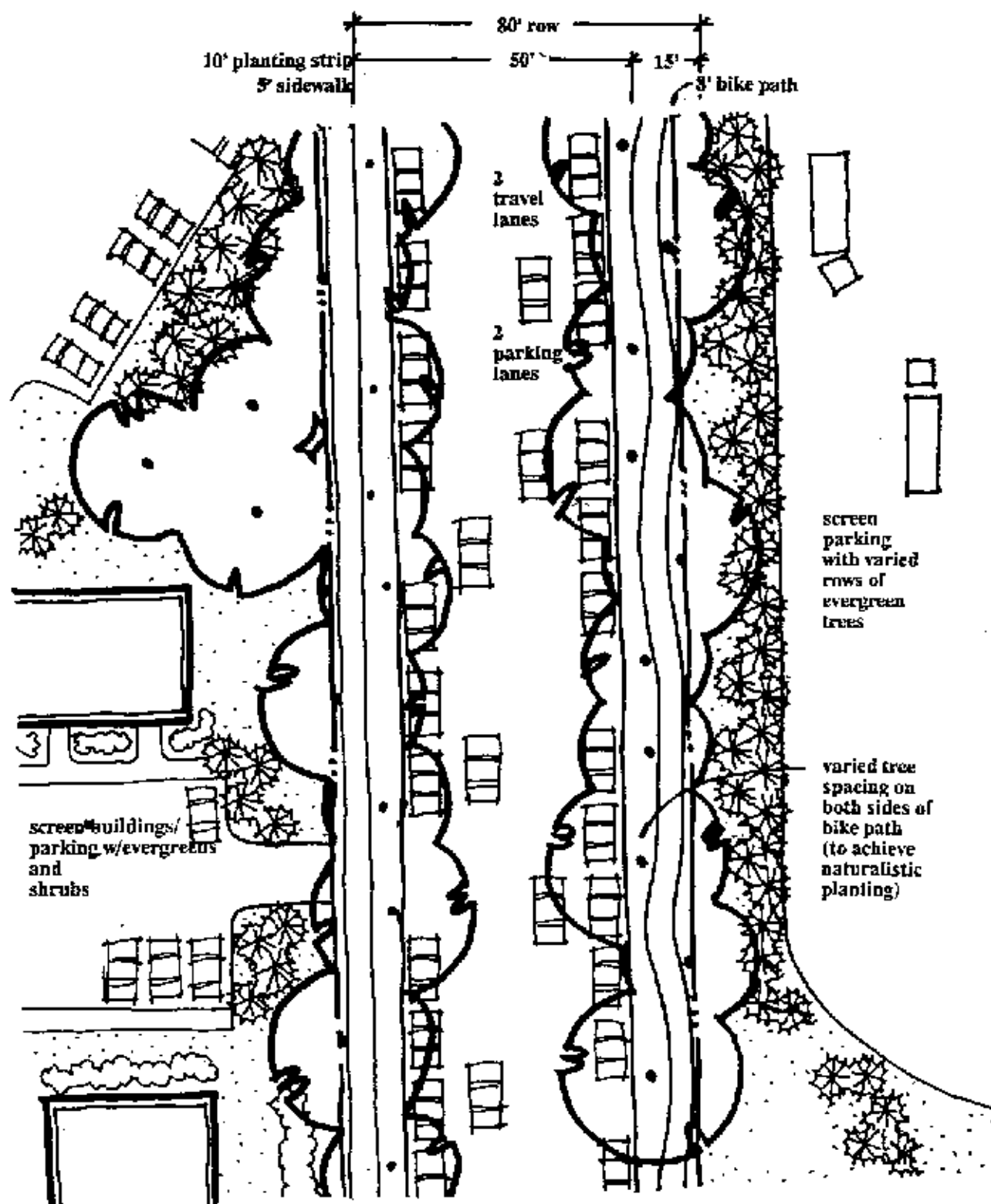
Wisteria Drive, from Father Hurley Boulevard to Great Seneca Highway, is a four-lane roadway with no median within an 80-foot right-of-way. Sidewalks, five feet wide, are proposed to be set back from the curb eight to ten feet on both sides. Currently, Sugar Maples are planted 60 feet on center on both sides of a majority of Wisteria Drive (between Father Hurley Boulevard and MD-118). New Sugar Maples are proposed to be planted to establish a 30-foot spacing. On the east side of MD-118, existing oaks will be supplemented to achieve a similar spacing.



WISTERIA DRIVE

## LOCBURY DRIVE

Locbury Drive will provide a connection for local traffic between the Town Center Core (TC-1) and TC-5, and is also proposed to serve as a park-like pedestrian and bike route. It consists of a 70-foot right-of-way and, in accordance with the Germanstown Master Plan, will have four lanes with no median. The outside lanes are proposed to be used for parking. A pedestrian path, six feet in width, will be set back ten feet from the curb on one side and a bike path, eight feet in width, will be incorporated on the other side. Street trees will be planted between these pathways and the roadway. Setbacks and spacing may vary. Buildings are recommended to be set back so as to allow space for park-like landscaping beyond the sidewalk.



LOCBURY DRIVE - SPECIAL STREET

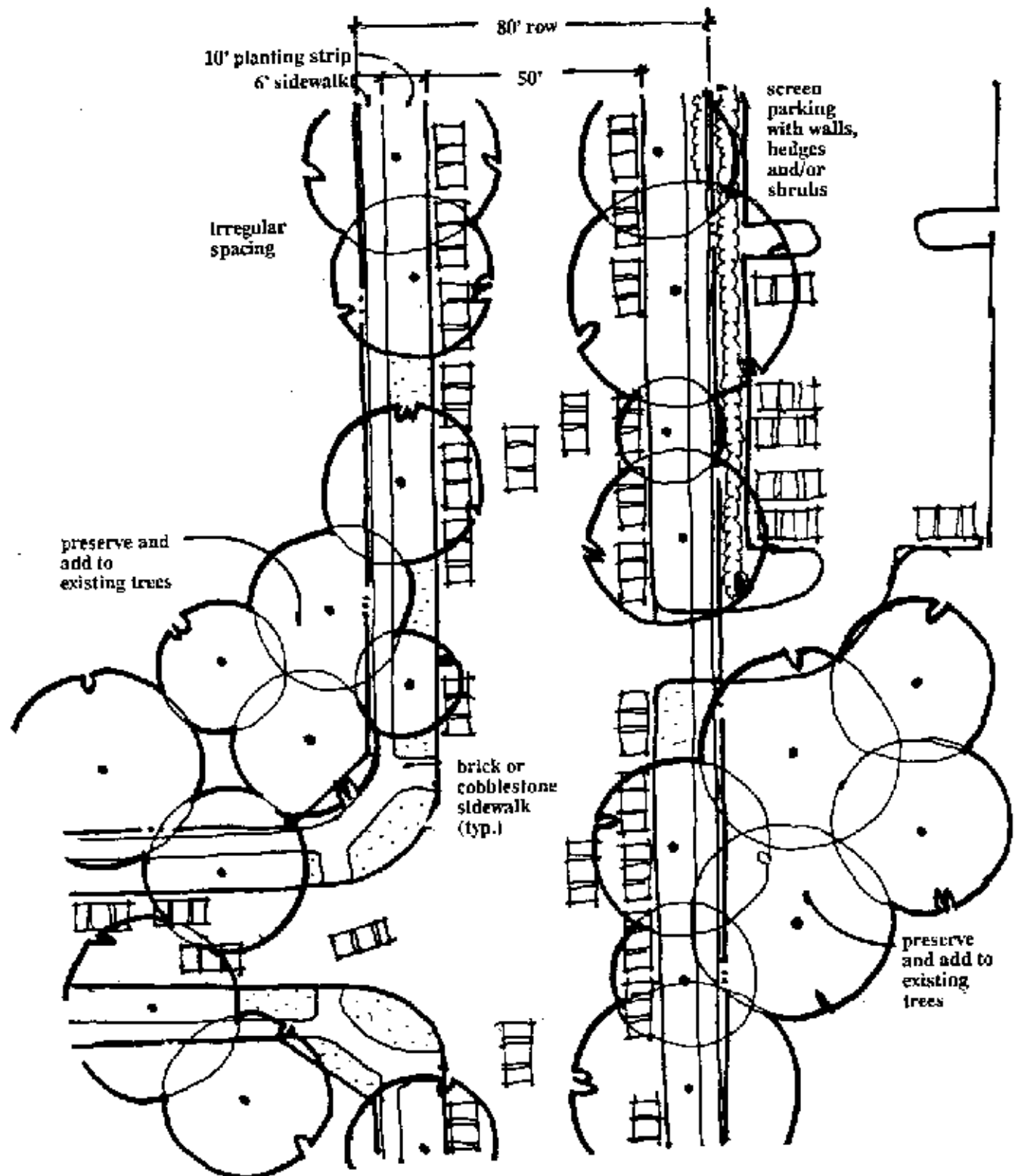
**WALTER JOHNSON DRIVE  
(OLD MD-118)**

The Germantown Master Plan recommends that Walter Johnson Drive (Old MD-118) end in a one-way, two-lane loop road approximately 750 feet southwest of Wisteria Drive. The bridge across the CSX Railroad tracks is proposed to be closed to vehicular traffic. This design study recommends connecting Old MD-118 to new MD-118 with a two-way, four-lane road, to intersect at the proposed Waters Road entrance to TC-5. This connection should have a 100-foot right-of-way. Another parking facility (lot or garage) is proposed to include this roadway and land on both sides. This would preclude connection from MD-118 to Old MD-118 and the MARC station. It is recommended that, if feasible, this facility be located to the northeast of Waters Road so as to allow direct access from MD-118 to both the existing and proposed MARC parking facilities and the station as well as Old MD-118.

Old MD-118 is a four-lane roadway (two moving lanes) with a 100-foot right-of-way. Six-foot-wide sidewalks on both sides are proposed to be set back ten feet from the curb. Street trees will be planted at varying spacing on both sides between the sidewalk and the roadway to enhance the character of this informal open space link.

The portion of Walter Johnson Drive (OLD MD-118) to the north of Wisteria Drive is approximately 750 feet long, with a right-of-way of 80 feet. The roadway ends at Germantown Square Park. The Master Plan recommends four lanes with sidewalks and street trees on both sides. The outside lanes are designated as parking lanes. A six-foot-wide sidewalk is recommended to be located ten feet from the curb on the north side of this road and a pedestrian path is proposed for the south or Northlake side. This trail may vary in its distance from the street. Street trees are recommended to be planted at varying spacing on both sides of the street.

The historic character desired for this street is to be created by the use of lighting, sidewalk paving, and street furniture similar in style to that of the recently completed MARC Commuter Rail Station. The street may also incorporate special paving materials.



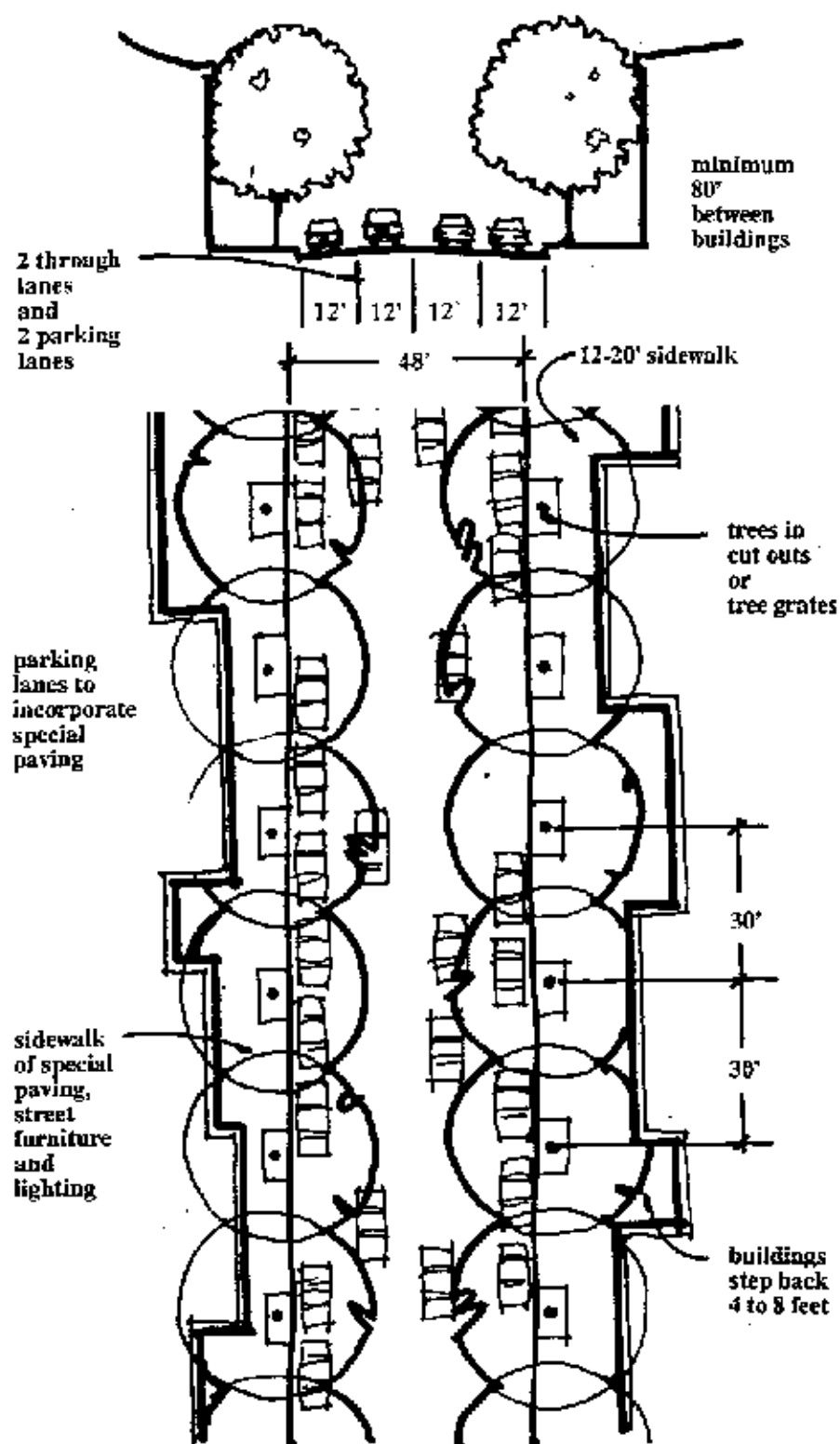
WALTER JOHNSON DRIVE (OLD MD-118)

**MAIN STREET (TC-1)**

The richest concentration of special paving, lighting, street furniture, and landscaping will be incorporated into the Town Center Core and Main Street. The recommended streetscape is designed to promote and enhance pedestrian use.

**TYPICAL** Main Street will typically maintain a distance of between 70 and 86 feet between buildings. The street incorporates four lanes, two of which will primarily serve as curbside parking. These lanes are recommended to incorporate special paving. Sidewalks, extending from the street to the face of the buildings, are proposed to vary from 12 to no more than 20 feet in width. Areas of planting are recommended to be incorporated as appropriate to the adjacent development. Trees of a pedestrian scale are to be planted 30 feet on center on both sides of the street in either planting beds or planters. The Main Street entrances to TC-1 at Middlebrook Road and Crystal Rock Drive each are recommended to incorporate entrance features and special landscaping.

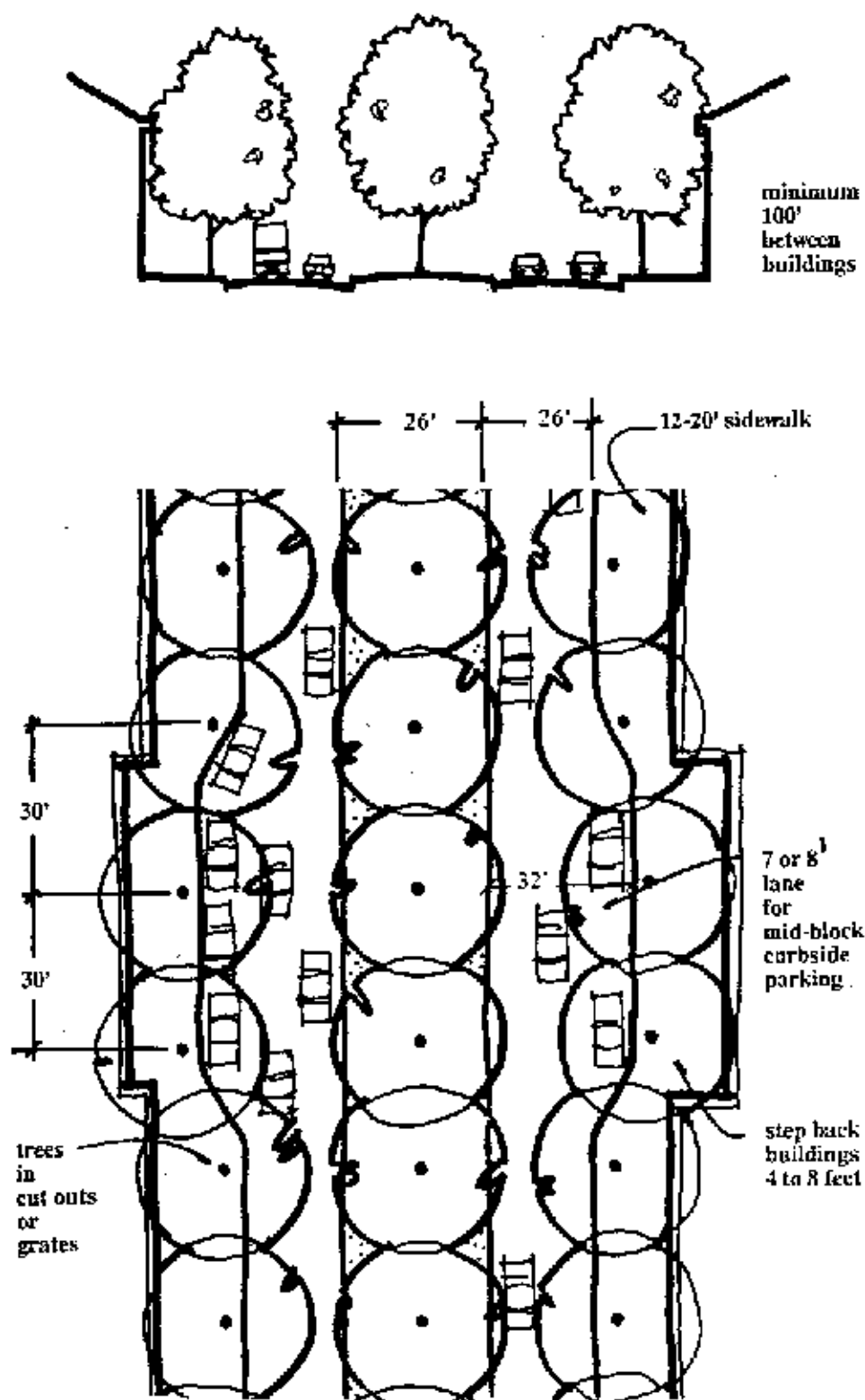




MAIN STREET (TC-1)

## TOWN CENTER BOULEVARD (TC-1)

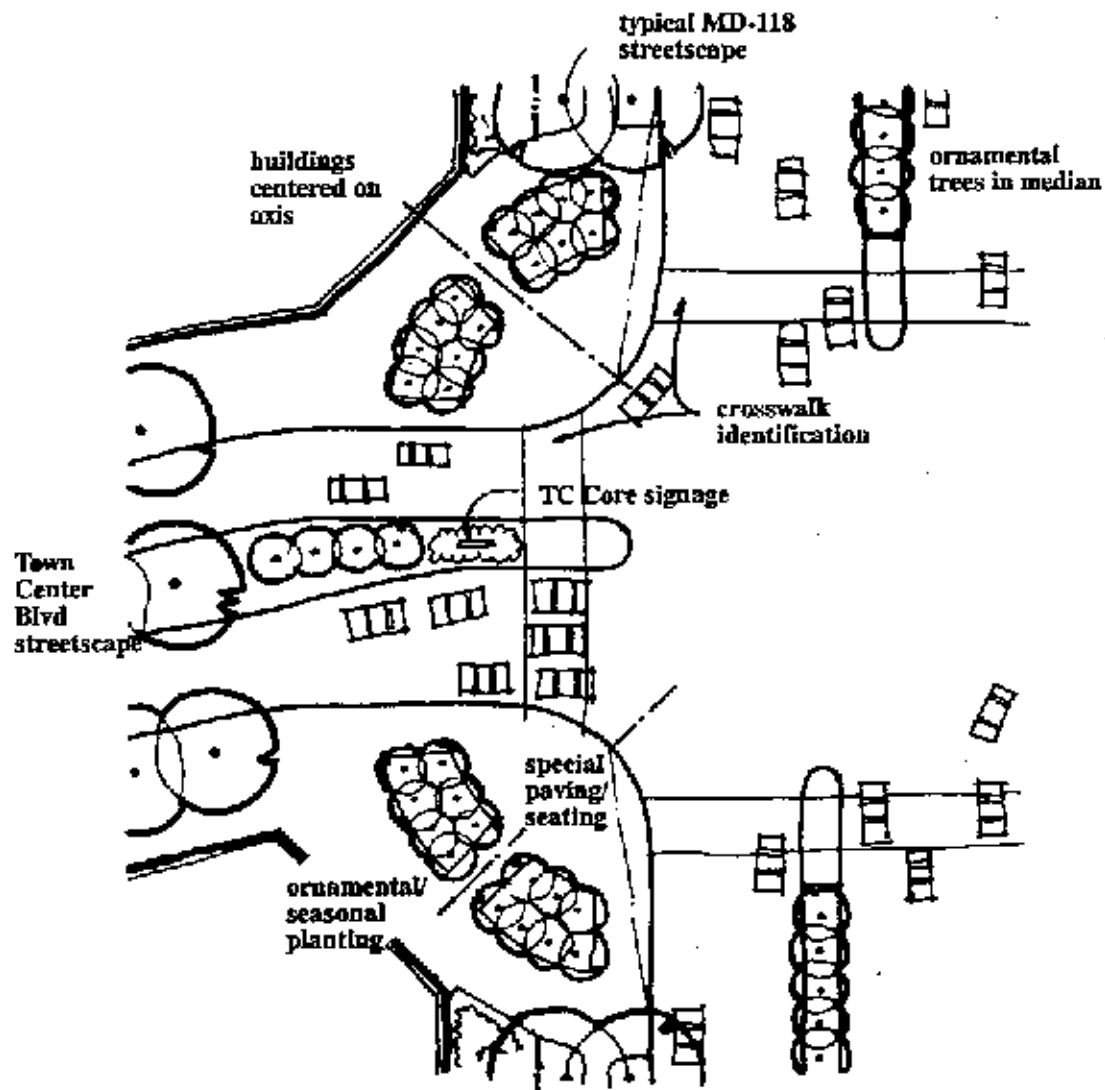
**TYPICAL SECTION** The boulevard connecting Main Street and the MD-118 entrance to the Town Center Core will typically have a distance of 106-116 feet between buildings and incorporate four traffic lanes, a 26-foot-wide median, and a 12- to 16-foot-wide sidewalk. Curbside parking will be provided mid-block in cut-out bays. Street trees are to be planted 30 feet on center on both sides of the roadway, as well as in the median.



TOWN CENTER BOULEVARD (TC-1)

**ENTRANCES**

The Town Center Boulevard entrance is recommended to have an enhanced landscape treatment similar to the other major MD-118 corners. Ornamental trees are to be arranged in a pattern to serve as a backdrop for the pedestrian zone at the street corners. Ornamental trees will also be planted in the median.

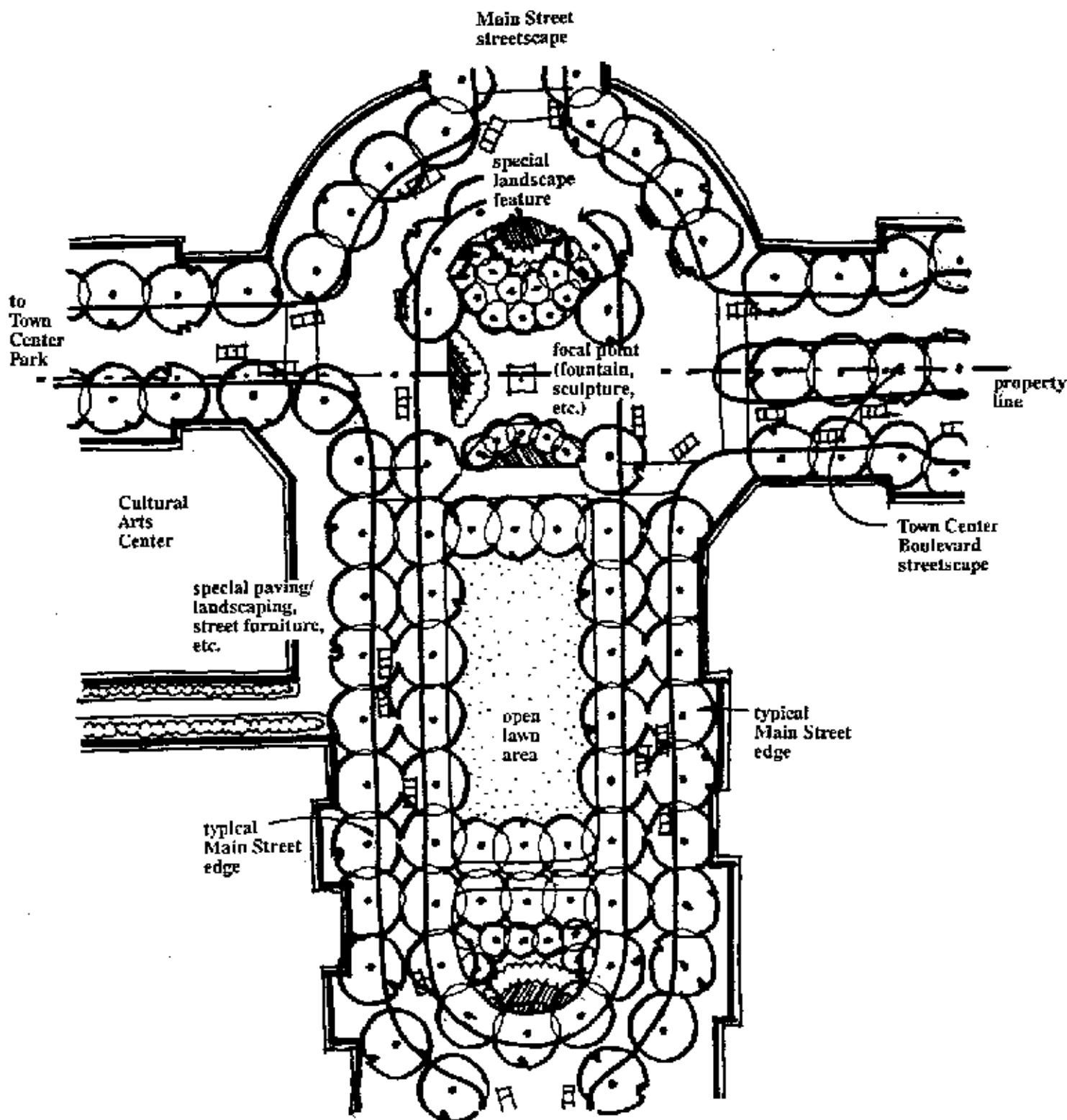


## TOWN CENTER COMMONS (TC-1)

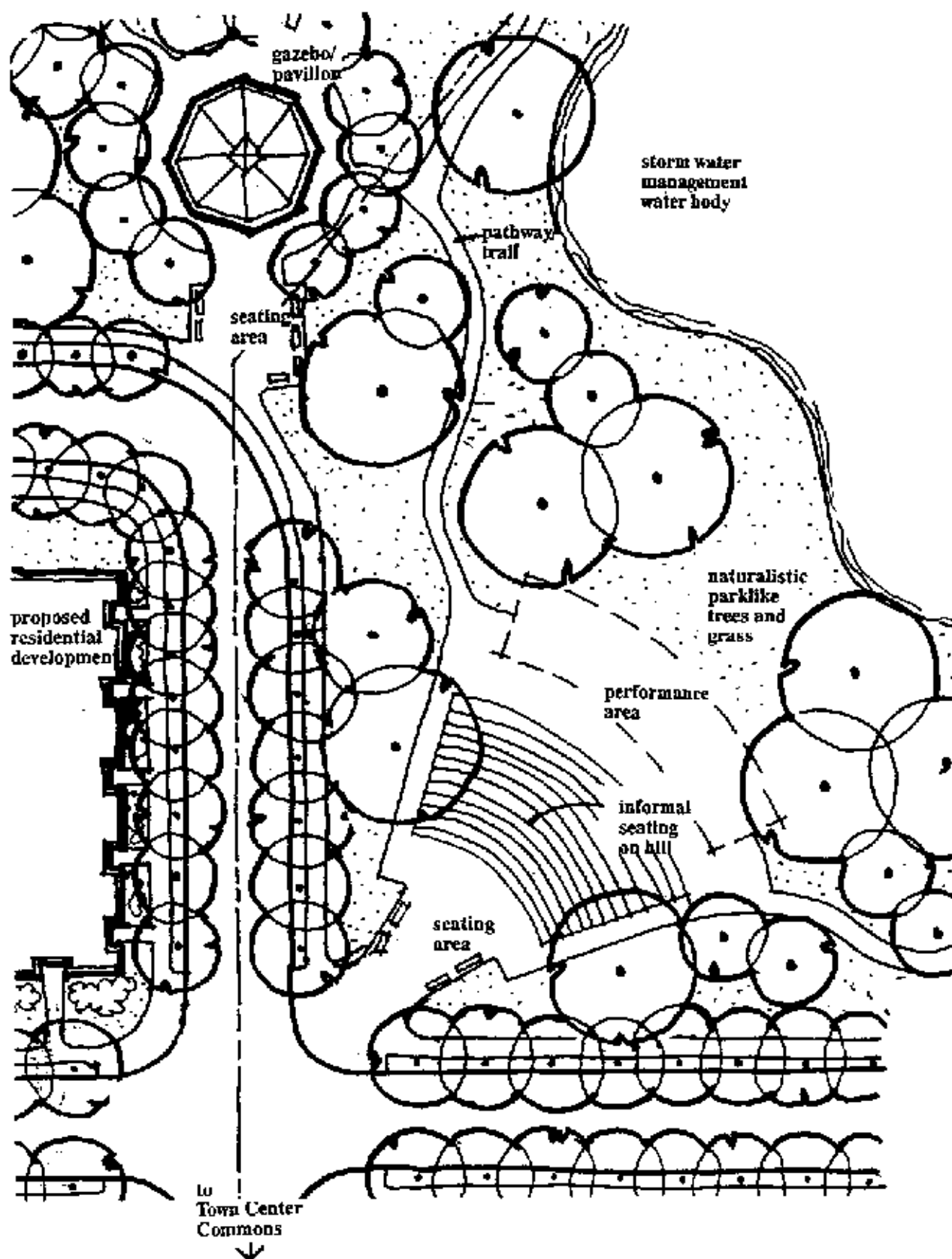
Town Center Commons is the central urban open space in the Town Center Core. Buildings facing onto the commons are proposed to be from 176 to 184 feet apart. The Commons is proposed to be 112 feet wide. Street trees are to be planted 30 feet on center, set back three feet from the curb, within six-foot cut-outs in a 12-foot-wide sidewalk. Landscaping is recommended to include shade trees, ornamental trees, and gardens, as well as a significant lawn area. Walking, sitting, and outdoor events can be accommodated within this Commons.

## TOWN CENTER PARK (TC-1)

Town Center Park is proposed to incorporate one or more ponds, a natural wetlands area, and areas for outdoor gatherings and passive recreation. An amphitheater is also proposed. Along the perimeter of the park the streetscape is recommended to consist of a four- or six-foot-wide sidewalk set back seven feet from the curb, with street trees planted 30 feet on center, between the sidewalk and the curb. Pathways will lead into and through the parkland.



# TOWN CENTER COMMONS (TC-1)



TOWN CENTER PARK (TC-1)

## INTERNAL STREETS (TC-1)

### PARKING ACCESS

Streets between commercial buildings and surface parking areas are proposed to have two through lanes and one seven- or eight-foot-wide discontinuous parallel parking lane on the side of the street adjacent to the building(s). Buildings are recommended to be set back 16 to 20 feet from the curb. This setback will allow a sidewalk with street trees planted in cut-outs, and up to a ten-foot-wide planting strip adjacent to the building.

Within the parking area, parking bay medians are proposed as typically six feet in width, also incorporating a 15-foot-wide median across from some building entrances, where possible. The six-foot medians would have a single row of shade trees, 30 feet on center. The 15-foot median would consist of a walkway, four or five feet in width, flanked by planting strips five or six feet in width, with shade trees 30 feet on center.

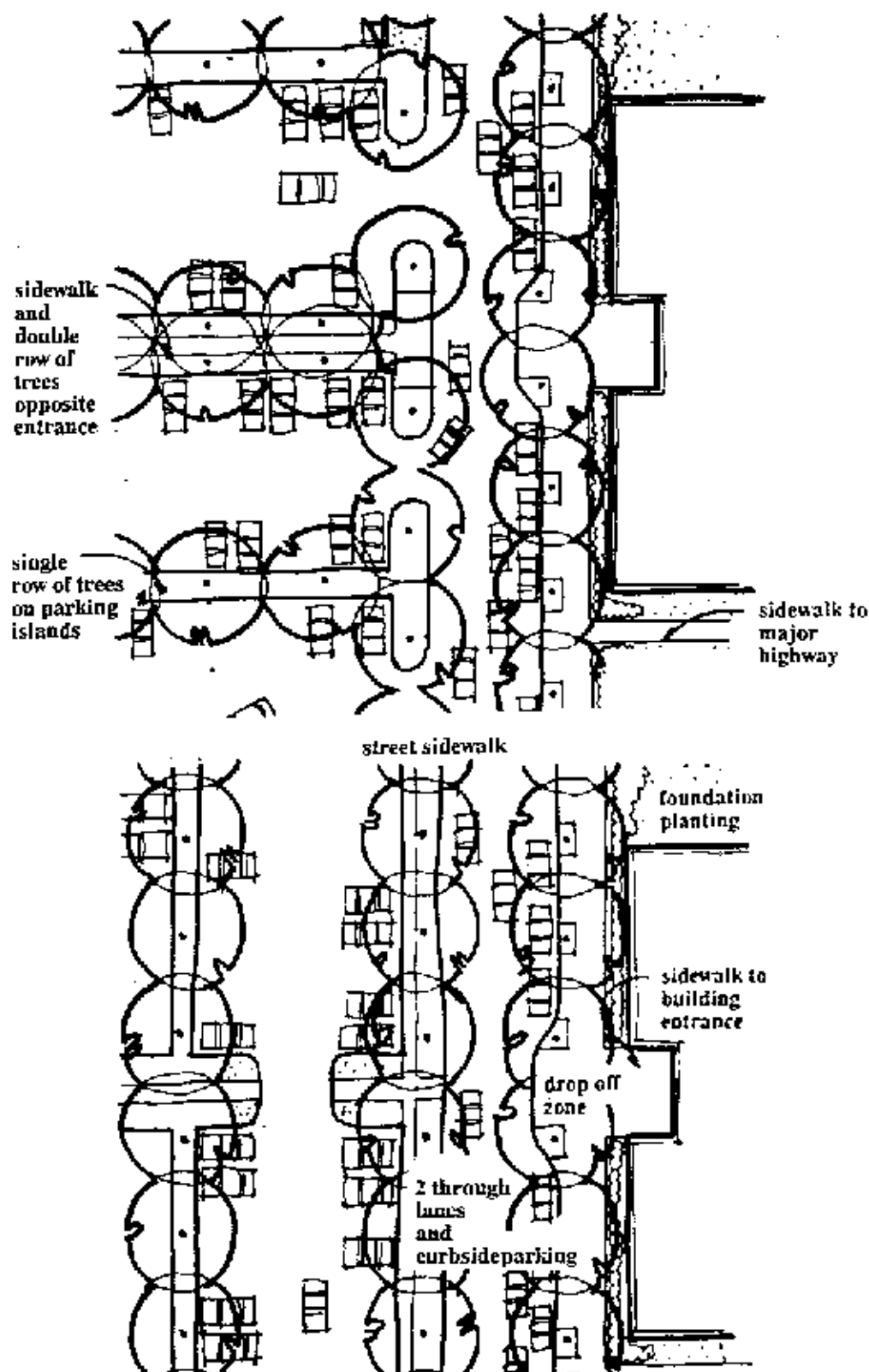
### PARKING EDGE

Streets between commercial buildings and parking lots and/or structures are proposed to have two 10-foot-wide through lanes and either one or two seven- or eight-foot-wide discontinuous parking lanes. Buildings will be set back 16 to 20 feet from the curb. This setback area will allow a sidewalk with street trees planted 30 feet on center in cut-outs, and a planting strip adjacent to the building.

Streets that occur between blocks of parking are proposed to have two moving traffic lanes and two discontinuous parallel parking lanes. Sidewalks are incorporated on both sides of the street. Curbs of the adjacent parking areas will be located five to six feet beyond the edge of the sidewalk. Sidewalks are flanked by street trees planted 30 feet on center.

The intent of this streetscape treatment is to provide the character of a real street in an area where parking flanks the street and to allow for the retrofit of the parking lot with buildings or structured parking.

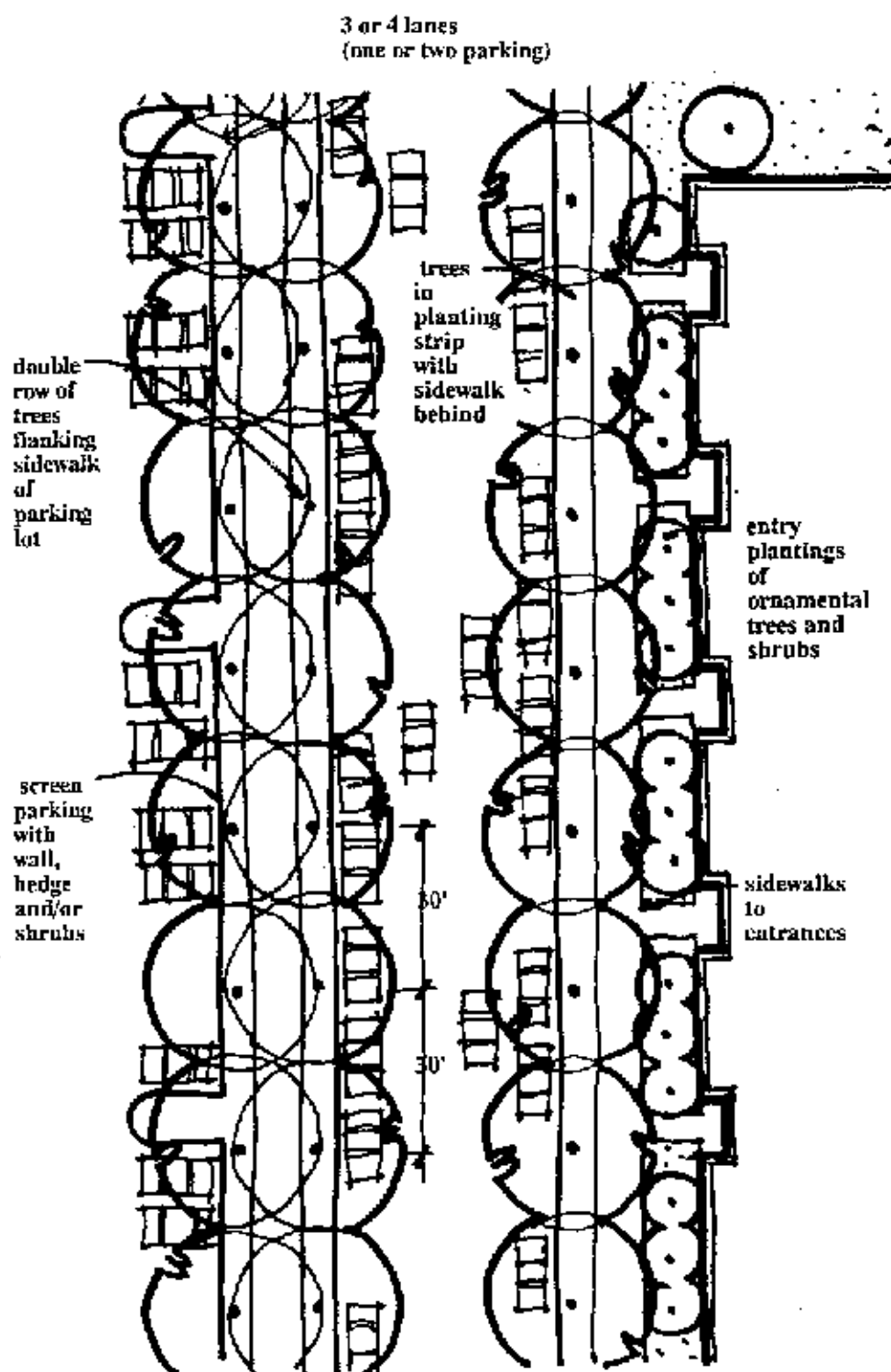




INTERNAL STREET (TC-1)

## RESIDENTIAL STREET (TC-1)

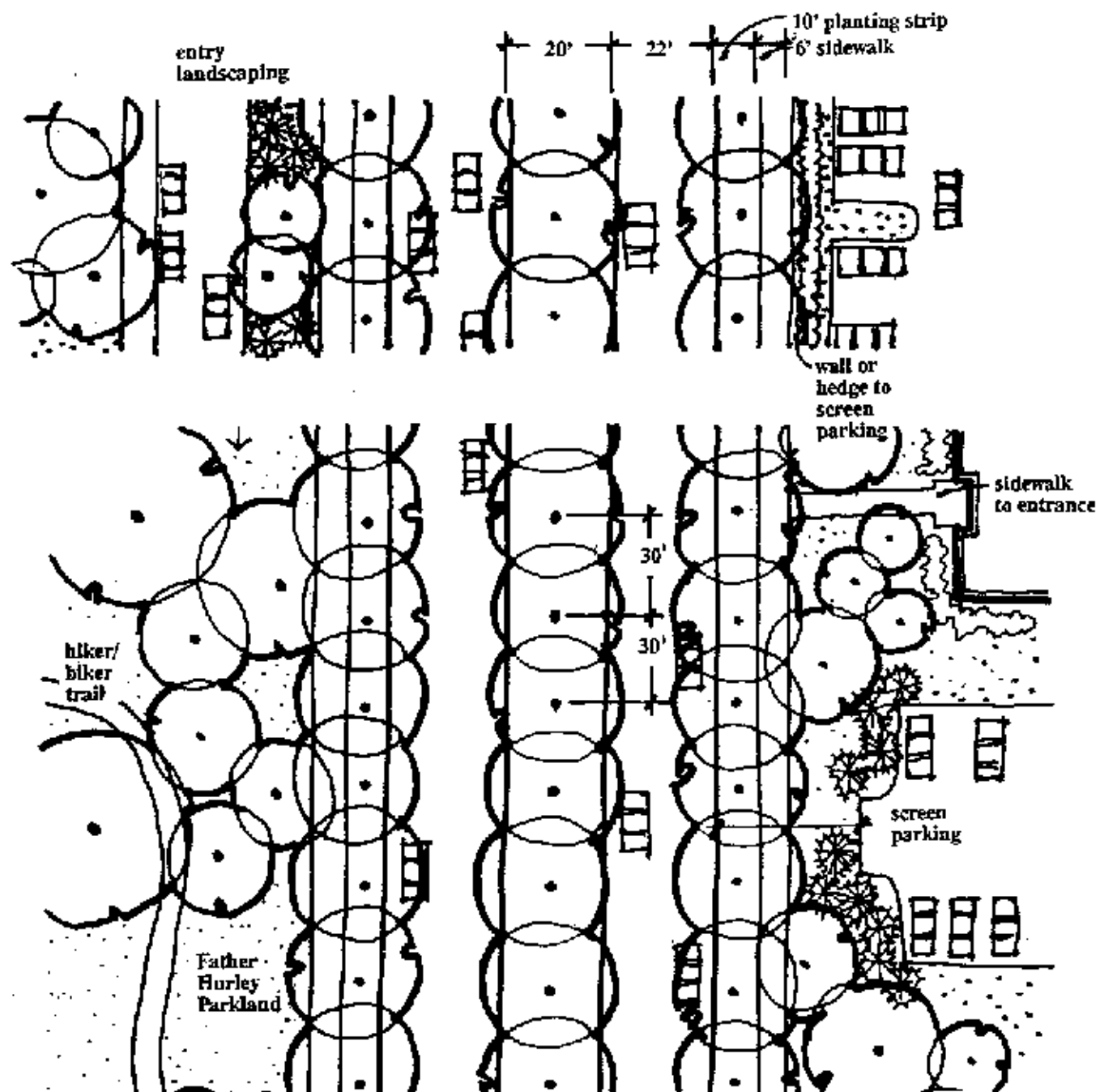
Streets between residential buildings and parking lots are proposed to have three or four lanes, one or two of which will be used for curbside parking. Residential buildings are recommended to be set back a minimum of 20 feet from the curb. The setback area will consist of a grass strip planted with street trees 20 to 30 feet on center, a four- to six-foot-wide sidewalk, and a minimum of a ten-foot-wide area for private entryway gardens. The strip between the street and the parking area is also 20 feet wide and will consist of a grass strip planted with street trees, a sidewalk, and a buffer strip incorporating shade trees between the sidewalk and the parking.



## BOULEVARDS (TC-5)

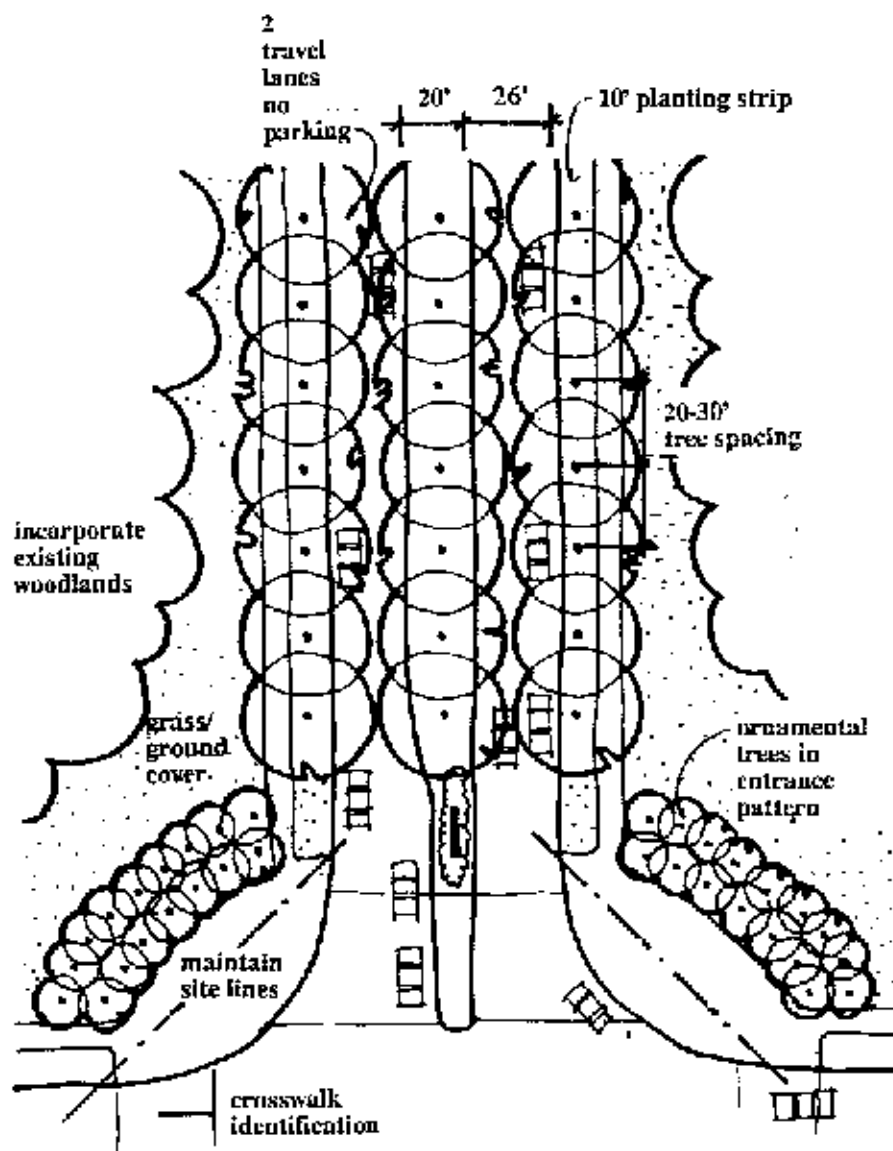
The boulevards connecting the entrances to TC-5 are proposed to consist of four lanes and 28- to 30-foot-wide medians.

**Typical Section (Residential and Parking Edges)** Six-foot-wide sidewalks will be separated from the curb by an eight-foot grass planting strip. Street trees will be planted 30 feet on center on both sides of the roadway and in the median. Where surface parking areas abut the roadway an additional seven- or eight-foot-wide strip, incorporating a wall or planted with shrubs or hedge, will separate the sidewalk from the parking. Where the TC-5 boulevard abuts residential, a substantial setback between the curb and the buildings is proposed to be provided. This area will be planted with a combination of evergreen trees, shade trees, and flowering trees, informally arranged to create a residential character. The opposite side of the boulevard will also be informal in response to the development of the proposed Father Hurley Park in this location.



## MD-118 ENTRANCE

The MD-118 TC-5 entrances will consist of plantings of ornamental trees set back from the corners to allow for an enlarged pedestrian area where sidewalks intersect. Signage, landscaping, and seasonal plantings are recommended to be incorporated into the median of the boulevard. Existing stands of trees will be preserved beyond the streetscape so as to retain the natural character of this area of Town Center (also the MD-118 **gateway** from the south and west).

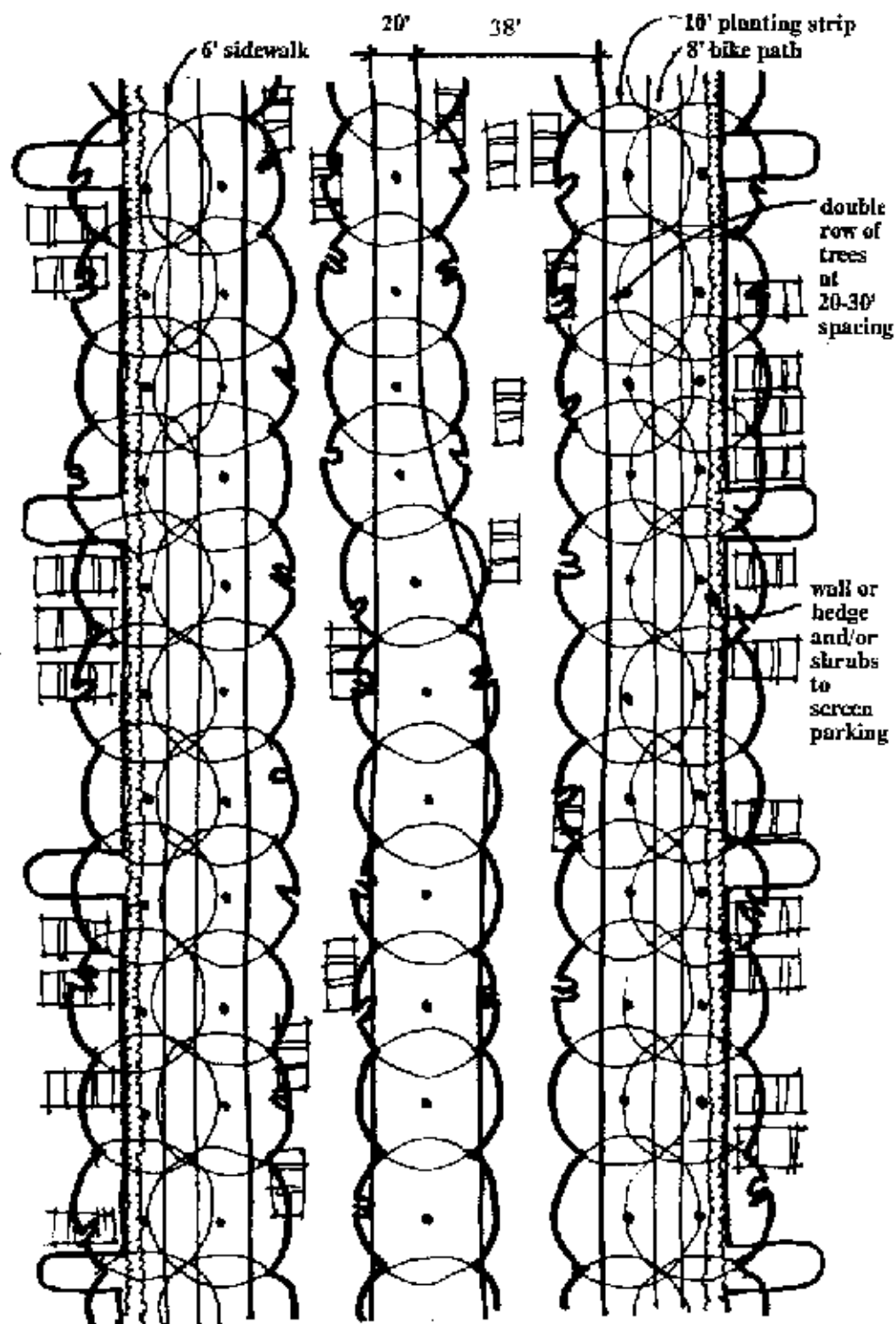


MD-118 ENTRANCE

## WATERS ROAD (TC-5)

The Waters Road boulevard incorporates a second row of trees on the parking lot side of the sidewalk on both sides. This, the major boulevard through TC-5, is differentiated from the other streetscapes in this Analysis Area as well as provided with more separation and screening from the adjacent parking. The southeast side of the roadway also continues the Locbury Drive special pedestrian street, incorporating the hiker-biker trail that links the TC-1 Town Center Core to the TC-6 Historic District.





WATERS ROAD (TC-5)