

Design  
Guidelines



January 2014

# Chevy Chase Lake Sector Plan



Montgomery County Planning Department  
M-NCPPC  
[MontgomeryPlanning.org](http://MontgomeryPlanning.org)

## Abstract

This document contains design guidelines that property owners, community members, and planners should use to implement the vision and recommendations of the 2013 approved and adopted Chevy Chase Lake Sector Plan.

## Source of Copies

The Montgomery County Planning Department  
The Maryland-National Capital Park and Planning Commission  
8787 Georgia Avenue  
Silver Spring, MD 20910

**Online at:** [MontgomeryPlanning.org/community/chevychaselake](http://MontgomeryPlanning.org/community/chevychaselake)



*Future development on Connecticut Avenue should enhance existing community character.*



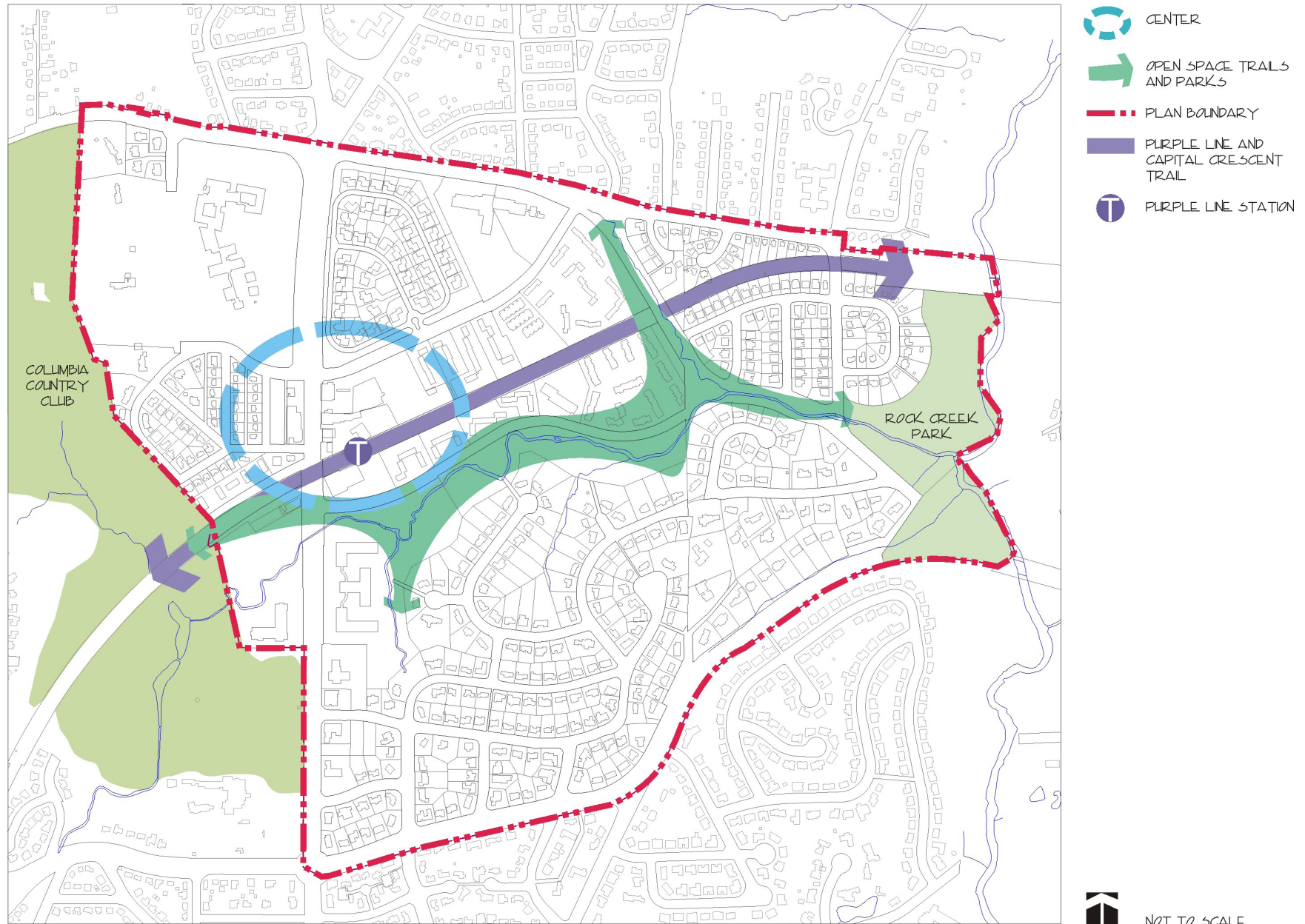
## Contents

<b>Introduction</b> .....	5
<b>Design Concepts</b> .....	7
Center.....	7
Open Space.....	9
Connections.....	11
<b>Criteria for Compatibility</b> .....	14
Scaled for People.....	14
<b>Guidelines</b> .....	25
Purple Line and Capital Crescent Trail.....	25
Connecticut Avenue Boulevard .....	27
New Street.....	32
Center.....	34
Chevy Chase Lake East Shopping Center.....	34
Chevy Chase Lake Apartments.....	37
<b>Other Sites</b> .....	39
Howard Hughes Medical Institute.....	39
Chevy Chase Lake West Shopping Center and Loughborough Place.....	41
8500 Connecticut Avenue (Arman's Chevy Chase Service Station).....	43
Newdale Mews.....	45
8402 Connecticut Avenue (Parkway Custom Dry Cleaning).....	47
8401 Connecticut Avenue.....	49
Tranquility Trail.....	51

## Maps, Sketches, and Illustrations

Map 1 Urban Design Framework.....	4
Map 2 Center Concept, Elements.....	6
Map 3 Open Space Concept, Elements.....	8
Map 4 Connection Concept, Elements.....	12
Map 5 Howard Hughes Medical Institute.....	38
Map 6 8500 Connecticut Avenue.....	42
Map 7 8402 Connecticut Avenue.....	46
Map 8 Tranquility Trail.....	50
 Sketch 1 Park Connection.....	9
Sketch 2 Purple Line Platform Sight Lines.....	25
Sketch 3 Underpass Visibility.....	33
Sketch 4 Central Open Space.....	35
 Illustration 1 Purple Line and Capital Crescent Trail, Section.....	24
Illustration 2 Connecticut Avenue, Section.....	26
Illustration 3 Connecticut Avenue Boulevard.....	31
Illustration 4 New Street, Detailed Section.....	32
Illustration 5 Chevy Chase Lake Shopping Center Section.....	34
Illustration 6 Chevy Chase Lake Apartments Section.....	36
Illustration 7 Chevy Chase Lake West Shopping Center, from Laird Place.....	40
Illustration 8 Chevy Chase Lake West Shopping Center, from Manor Road.....	40
Illustration 9 Newdale Mews, Section.....	44
Illustration 10 8401 Connecticut Avenue, Sections.....	48

**Map 1 Urban Design Framework**



## Introduction

The Chevy Chase Lake Design Guidelines represent the County's and community's design aspirations for Chevy Chase Lake. They further help maximize the return on the public's investment in the Purple Line through high quality design.

Design guidelines should be used as a resource by all stakeholders to enhance urban design quality. Urban design is concerned with the physical characteristics of an area and the elements that help shape it. Guidelines help implement plan recommendations by promoting design excellence for private and public properties and projects, and they encourage the creation of safe pedestrian environments and attractive gathering places.

The overarching urban design strategies described in the Chevy Chase Lake Design Guidelines will help coordinate the way development proposals affect physical features: the public faces of buildings, spaces for public use, and the streets, sidewalks, parks, and plazas that provide outdoor settings for everyday activities.

Design guidelines are approved by the Planning Board and illustrate how plan recommendations might be met. They also inform applicants and public agencies about design expectations and possible ways to accomplish them. They further provide staff with a framework within which projects can be reviewed and enhanced design and related amenities can be secured. Guidelines do not mandate architectural styles, they apply to both regulatory reviews and advisory reviews of public and private projects, and they are revised and updated as necessary.

In Chevy Chase Lake, urban design should seek to:

- create a vital, mixed-use center
- establish a network of paths and open spaces
- guide the design of the Purple Line Station so that its access can be fully integrated.

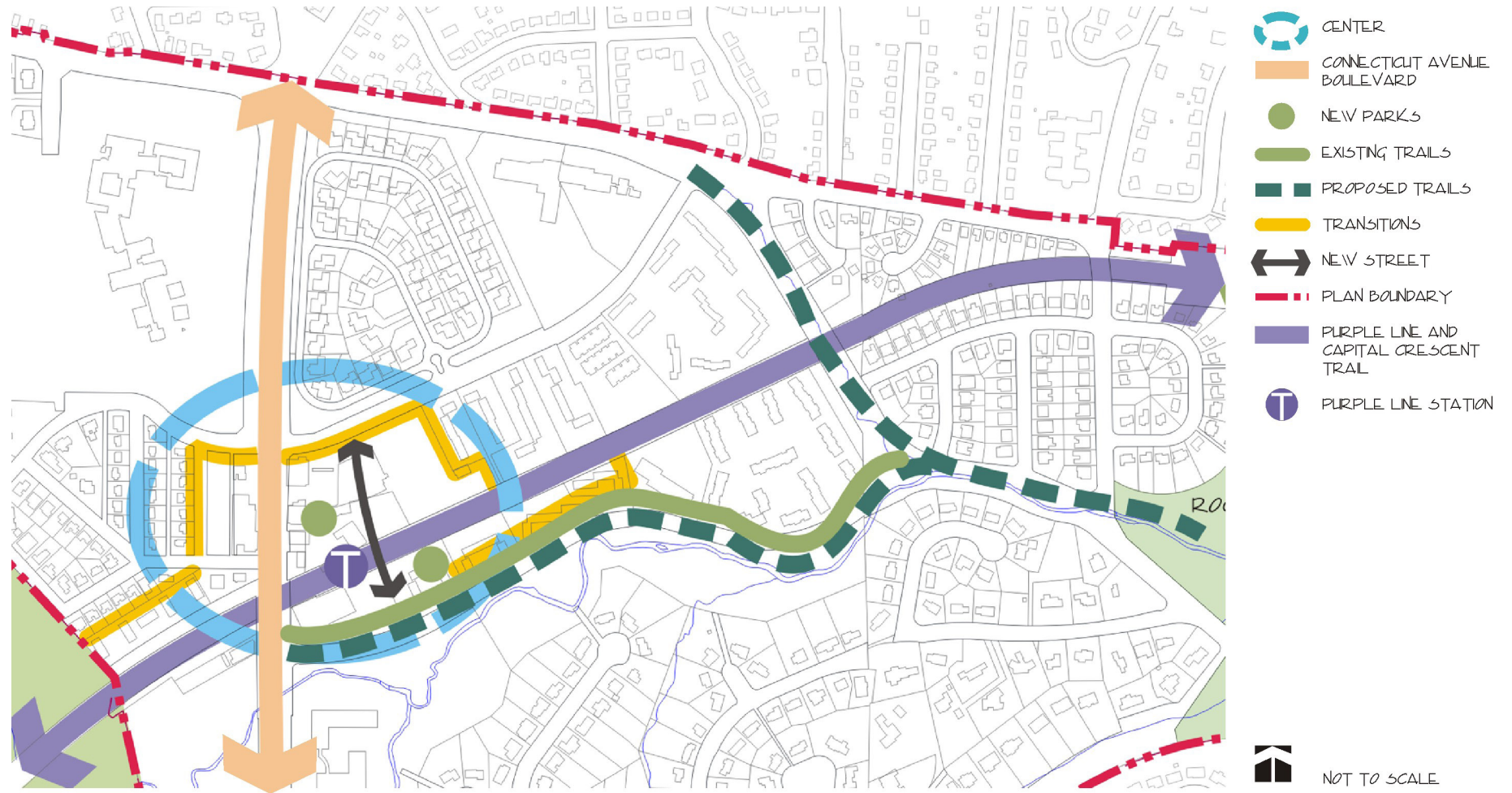
Accordingly, all projects in Chevy Chase Lake should comply with the design concepts, the criteria for compatibility, and the design guidelines that apply to specific properties.



*The central open space should be enlivened with engaging features to attract people.*



Map 2 Center Concept, Elements



## Design Concepts

The following design concepts are approaches that shape the overall form and function of Chevy Chase Lake. The key concepts are to: focus redevelopment to reinforce a center; integrate community open spaces, and ensure that all connections to the proposed Purple Line station are safe and visible.

### Center

Redevelopment in Chevy Chase Lake should result in a mixed-use, compact, and walkable Center. It should have intense uses and activities within walking distance of one another and a Purple Line station that is compatible with the existing community.

Design should create:

- a central open space and a new neighborhood park, connected by an open space system
- Connecticut Avenue as a boulevard—a street that unifies its east and west sides, lined with trees and buildings facing the street, and with new pedestrian crossings and gateways to mark entries into Chevy Chase Lake
- a new street (New Street) running north-south under the elevated section of the Purple Line, for pedestrians, bicyclists, transit users, and motorists
- trails and connections that create the Center as a crossroads for transit, walkers, and cyclists
- transitions to allow a seamless fit between new development and existing residential areas.

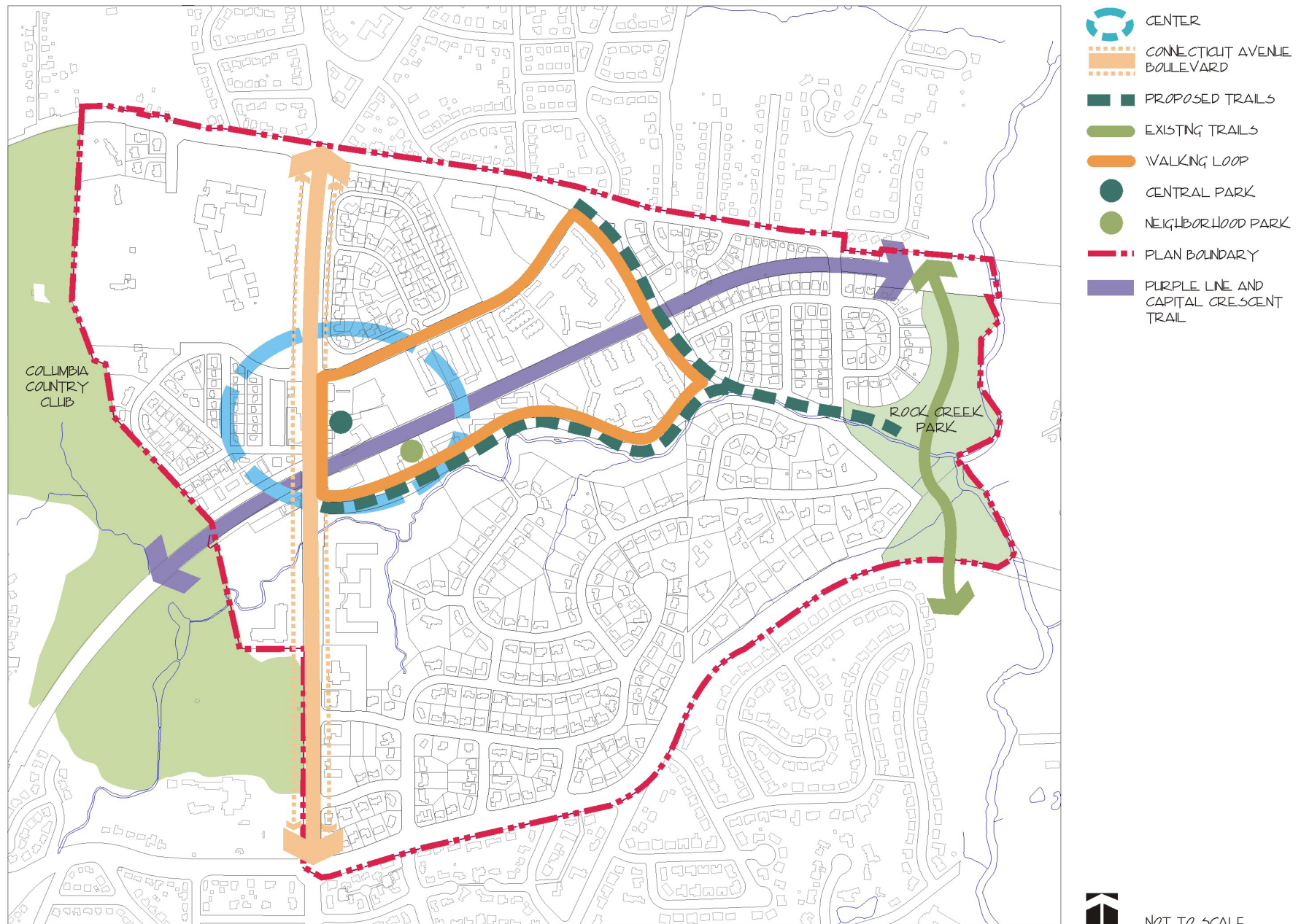
Design should consider:

- orienting buildings and open space to ensure the sun reaches the central open space and the Purple Line platform
- incorporating the local history into designs, including the area's establishment as a streetcar community
- sight lines among the central open space, the station platform, the mid-block crossing, and the new street
- how streets can be defined by buildings.



*Pedestrian crossings should be enhanced and include a variety of features .*

**Map 3 Open Space Concept, Elements**





## Open Space

The Center should also be shaped by a linked system of public and private open spaces and paths.

Design should create:

- a central open space
- a new neighborhood park with a playground and paths to adjacent communities
- a walking and biking loop trail that goes under the elevated section of the Purple Line
- connections from existing neighborhoods to the Coquelin Run Parkway Trail
- the Tranquility Trail along the Coquelin Run, connected to the Coquelin Run Parkway Trail.

Design should consider:

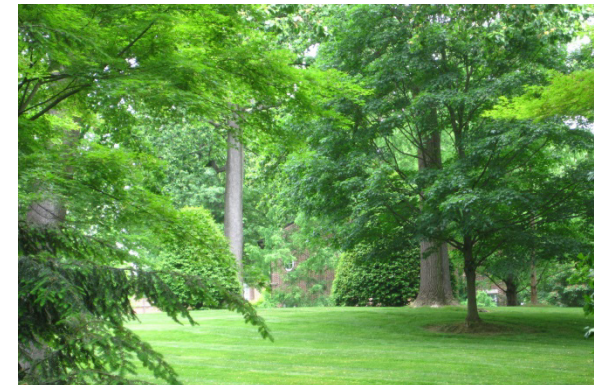
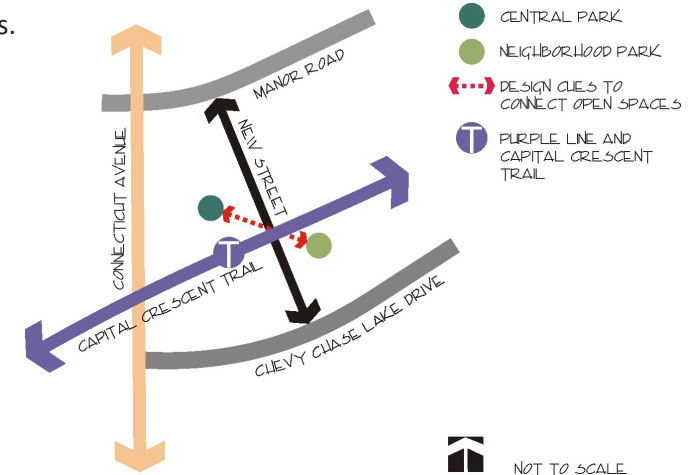
- streetscaping and seating that defines community character
- public art or markers that recount community history
- the area's green character and environmental features
- landscaping and design cues that visually connect the two parks.

The Center's few blocks are all near the Central Open Space or the new Neighborhood Park. Additional open space that may be required for a specific block will be determined at the time of development review.

The Central Open Space in Chevy Chase Lake will be privately owned, but should function as a public park, and be designed as a civic green. The civic green should be about  $\frac{1}{2}$  acre, contain formally planned, flexible, programmable spaces for informal gathering or special events and may support activities including open air markets, concerts, and festivals. A central lawn may be its focus with adjacent spaces providing complementary uses. It may also include gardens, water features, and shade structures.

The new Neighborhood Park should include some community recreation activities and also function as a green buffer between high density development and lower density residential areas. This park should provide a green space for residents and workers to relax and play and be at least  $\frac{1}{4}$  acre. It should further include landscaping, sitting/picnic areas, play equipment, courts, and shelters.

### Sketch 1 Park Connection



*The HHMI campus reinforces the green character of Chevy Chase Lake.*

Open Space Hierarchy	
PROS Plan Recommendations	Chevy Chase Lake Sector Plan Recommendations
<b>for the Sector Plan area</b>	
active recreation destinations within or near the plan area	Chevy Chase Elementary School Field, Chevy Chase Local Park, Ray's Meadow Local Park (in Rock Creek Regional Park)
central Civic Green Urban Park	new Urban Park at the Center and Purple Line station
interconnected system of sidewalks and trails	Capital Crescent Trail, Rock Creek Trail, Coquelin Run Parkway Trail, walking/biking loops, Tranquility Trail
<b>for each urban neighborhood</b>	
neighborhood green, urban buffer park, or community use urban recreational park	new neighborhood green urban park with playground (on the HOC properties)
<b>for each block</b>	
urban square, plaza, or green area	existing apartments open space and lawns
<b>for each residence</b>	
private outdoor space	yards of homes and townhomes, balconies and patios of apartments

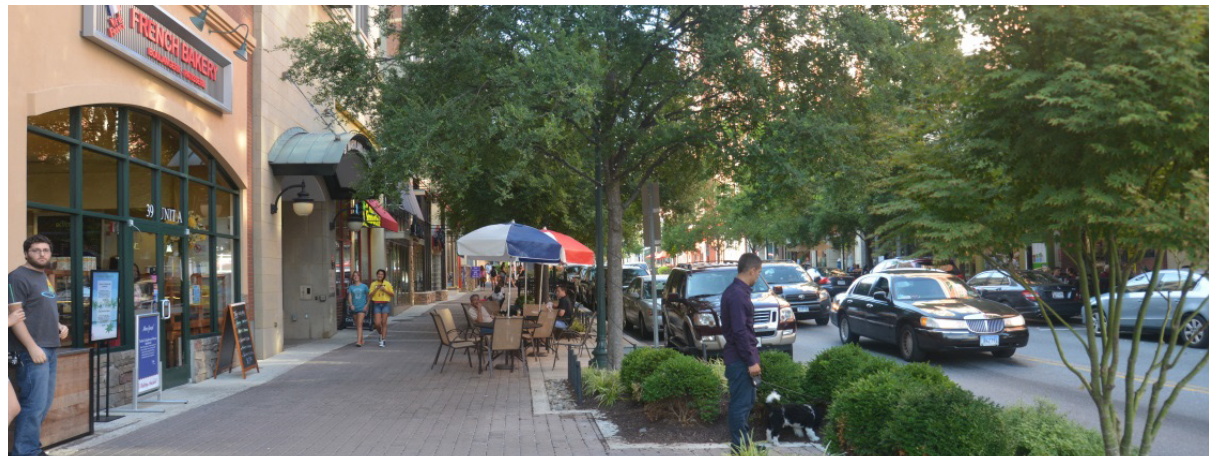
## Connections

Pedestrian-friendly streets are important to creating a walkable center and the guidelines are intended to enhance new and existing streets so they contribute to the overall identity of Chevy Chase Lake, improve walkability and bikeability, and increase tree canopy. The Plan and guidelines address both the function and character of varied connections.

As individual projects are designed and reviewed, the streetscape design will be determined in coordination with SHA and MCDOT, with each project contributing to an overall streetscape concept.

Design should create:

- Connecticut Avenue as a boulevard with a landscaped median that also serves as a pedestrian refuge
- a new street designed as a single environment that helps unify the Center and accommodates easy flow beneath the elevated Purple Line
- a pedestrian crossing of Chevy Chase Lake Drive and a sidewalk and signed shared bike route along it
- connections from the Purple Line station and Capital Crescent Trail, along both sides of New Street
- walking routes to the Neighborhood Park
- a connection to the Capital Crescent Trail on the east side of New Street that avoids street crossings.



*Provide adequate space not only for people, but all elements that shape a well-designed public realm.*

*Connections to the new neighborhood park should be pleasant and easy to navigate with small children.*



*Areas below the Purple Line should be designed considering access to light, air circulation, and streetscape continuity.*



**Map 4 Connection Concept, Elements**



Design should consider:

- the character of existing streets and trees
- the location of utility lines
- implementation and maintenance costs and responsibilities
- adequate space for pedestrians and street furniture, including:
  - street trees within curbside landscaped tree panels
  - ornamental street lights
  - consolidated street signs
  - taxi and bus pick-up/drop-off areas
  - a paved curbside edge to accommodate transit passengers and those using on-street parking
  - seating
  - display space for businesses
  - bike racks
- street furniture, which must also be approved by MCDOT and SHA, that creates a unique, Chevy Chase Lake design character.

*Designers should refer to the Bethesda Streetscape Plan, Technical Manual (May 2007) for information on appropriate street trees, planting standards, and paving details; however, elements should vary, subject to agency approvals, to create a design unique to Chevy Chase Lake.*

*The complete streets concept seeks to ensure that streets serve all users, cars as well as transit, cyclists, and pedestrians, in safe and pleasant environments. In Chevy Chase Lake, new and existing streets should incorporate design features that minimize adverse environmental impacts and accommodate new technologies.*

## Criteria for Compatibility

The Plan outlines three criteria to ensure that new development is compatible with the existing community (See Plan page 28). New projects that incorporate the criteria into their design will help Chevy Chase Lake grow while retaining its distinct character and identity. A commitment to high quality design will help maximize the return on the public's investment in the Purple Line.

### Scaled for People

Today, the scale of Chevy Chase Lake is one of its most recognizable features. Its current shopping area is relatively small and walkable. Its buildings are typically no more than three stories with frequent shop entrances and windows.



*The human scale of development , especially at the street level, is an important design consideration.*

### Buildings

- Make new buildings seem smaller, especially when viewed from across a street, the central open space, or a private yard by:
  - using deeper colors, richer materials, contrasts, raised and recessed patterns and forms, and design details to draw attention to lower levels
  - using lighter colors at upper levels so buildings better blend with the sky
  - setting back upper levels so they are less visible and imposing
  - using strong horizontals to make buildings seem shorter
  - manipulating massing to create a stepped building profile
  - composing buildings as a series of smaller forms.



*Horizontal bands can help de-emphasize building height.*



*The human scale of development, especially at the street level, is an important design consideration.*



*Ample windows at the sidewalk level, careful use of color, and building step-backs help create a comfortable scale.*



*A stepped roofline and light colors near the top can help reduce the mass of a building.*



*A varied façade line can add interest and help reduce the bulkiness of a building.*





*A well-designed open space is shaped by buildings, plants, seating areas, and the ground plane.*

## Open Spaces

- Create comfortably sized and furnished open spaces that are visible and inviting.
- Consider the pattern and materials of walkable surfaces, the placement of benches and plantings, and contrasting roofed or canopied spaces, with areas open to the sky.
- Avoid steps or grade changes at entrances.
- Design walls, water features, benches, and artwork to be visually interesting and touchable.
- Select aromatic plants that are visually interesting and touchable.
- Provide seating to accommodate individuals and groups.
- Design and locate steps or terraces that are comfortable to use.

*An overhead structure can help create a sense of enclosure.*



*Public spaces should have seating options like terraces, steps, and walls.*



*A tree canopy can help define an outdoor space.*



*Design details should invite interaction.*



## Streets

- Line streets with buildings to define the space and explore ways shorten crossing and walking distances.



*Wide streets can be tamed to feel comfortable - street trees, medians, and a well-designed architectural edge can help create an effective pedestrian scale.*



## Tradition(al)

New development should consider Chevy Chase Lake's setting and history. Buildings should interpret traditional, architectural forms.



*Hayes Manor, a local historic site*



*The Howard Hughes Medical Institute uses traditional architectural elements*



*This traditional arched window design could be interpreted and used in new buildings.*

### Materials

- Use or reference traditional materials such as brick, stone, painted trim, wrought iron, and weathered copper.

### Composition

- Interpret the character of Chevy Chase Lake's architectural composition.



*A gable and window pattern with traditional symmetry that are part of an asymmetrical composition.*



*New forms can be integrated into a traditional architectural composition.*

### Design Details

- Include interpretations of traditional design details, many of which are originally of wood or carved stone.

### Community History

- Feature local events, places, and history in the design of site features such as walls, pavement, signage, and furnishings.



*Art in a paving design can tell a story. Artist: Carolyn Braaksma*

### Nature and the Garden

Chevy Chase Lake is shaped by its rolling terrain, streams, and wildlife habitats. The groomed landscapes incorporate nature into a cultivated setting. Gardens include both native and non-native plants and are varied and complementary. They include the green lawns and mature trees of the Manor Road apartments, the campus of the Howard Hughes Medical Institute, and the leafy gardens and lawns of surrounding neighborhoods





*"Nature and the Garden" seeks to reflect the character of local neighborhoods.*

### Plants

- Use trees, shrubs, and plants that reference existing woodlands, favoring natives for their ability to thrive and their value to wildlife.
- Add and group trees and shrubs to screen, buffer, and beautify—particularly around parking structures, within parking lots, on road medians, and along street edges.
- Plan a succession of trees to replace mature, aging, and diseased street trees by planting new ones before the old ones die.



*A naturalized landscape helps the Howard Hughes Medical Institute fit into the surrounding neighborhood.*



*Street and sidewalk planting can extend nature into the built environment*



*Median planting should shade the streets but also ensure visibility*



*Native flowers can enhance the landscape*



## Water

- Consider water features that reference the Coquelin Run and its role in the community.

*A water feature referencing a flowing stream in an urban setting.*



*Coquelin Run in its natural setting*



*Coquelin Run*



*A water feature referencing a creek*





The choice of materials can create unity and cohesion between new development and the existing community

### Natural Materials for Site Features

- Incorporate local, natural materials for their character.

### Views

- Use the area's terrain to framing views from Connecticut Avenue, the Purple Line station platform, and the Capital Crescent Trail Bridge.



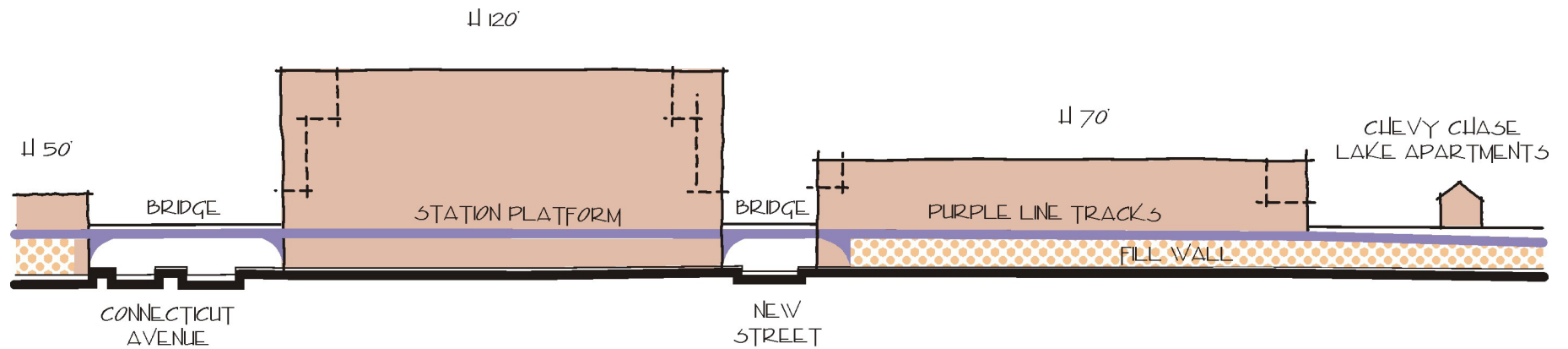
*When looking south, topography helps draw attention to the design significance of the Purple Line and Capital Crescent Trail bridges planned over Connecticut Avenue.*



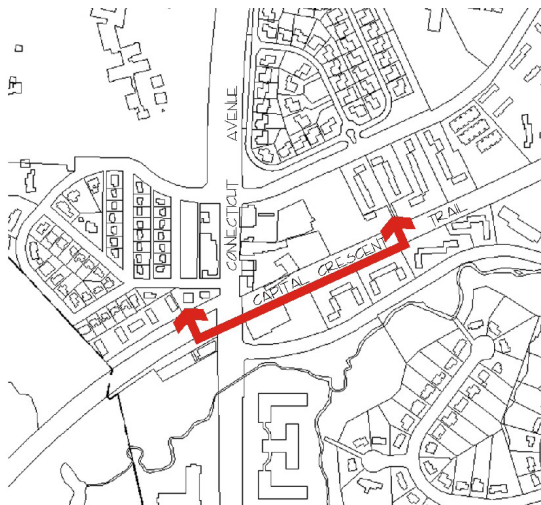
*The design of the Purple Line and Capital Crescent Trail must also consider views from the country club and surrounding neighborhoods.*



## Illustration 1 Elements Purple Line and Capital Crescent Trail, Section



THE DEPTH AND LOCATION OF BUILDING STEPBACKS WILL BE DETERMINED AT DEVELOPMENT REVIEW. THE STEPBACKS SHOWN HERE BY DASHED LINES ARE ILLUSTRATIVE.



LOCATION MAP SHOWS EXISTING CONDITIONS

## Guidelines

### Purple Line and Capital Crescent Trail

#### Design Intent

The station, bridges, and walls that support the transit line and Trail through Chevy Chase Lake should be designed as community landmarks that make the Trail enjoyable to use.

#### Purple Line Station

- The station should be visible from New Street, the Central Open Space, and Connecticut Avenue.
- The station should include distinctive elements that relate to the Chevy Chase Lake community and its history.

#### Bridges, Approaches, and Underpasses

- The transit and Trail bridges should be designed as community landmarks and should be distinctive, whether seen close up or from a distance. The bridges should further be viewed as sculptural elements and designed to reflect local character and incorporate art.
- The bridge approaches—stairs, ramps, escalators, and elevators—should be visible and accessible to all users. They should open directly onto public spaces and streets.
- Good visual connections should be created between the station platform and central open space, including wayfinding signs, paving patterns, and other visual cues.
- The elevated sections of the Purple Line and Trail should have multiple underpasses to allow safe, comfortable, and convenient walking, biking, and driving, particularly between Connecticut Avenue and the New Street. The underpasses should:
  - be well lit and accommodate the full width of the rights-of-way
  - have protected sidewalks on both sides that are set back from the curb
  - apply Crime Prevention through Environmental Design (CPTED) principles
  - incorporate decorative design details and art.

A fill wall in these locations is not an optimal solution, and is not recommended.

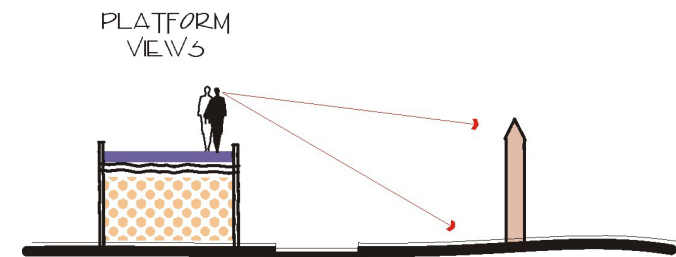


*The passages under the bridges should ensure visibility from within and from the outside, and be designed to ensure access and safety.*



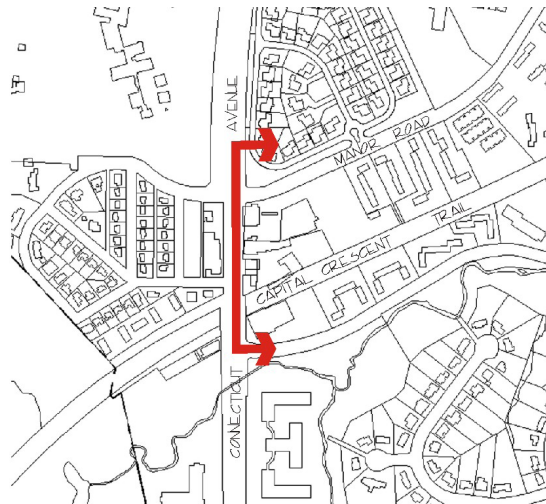
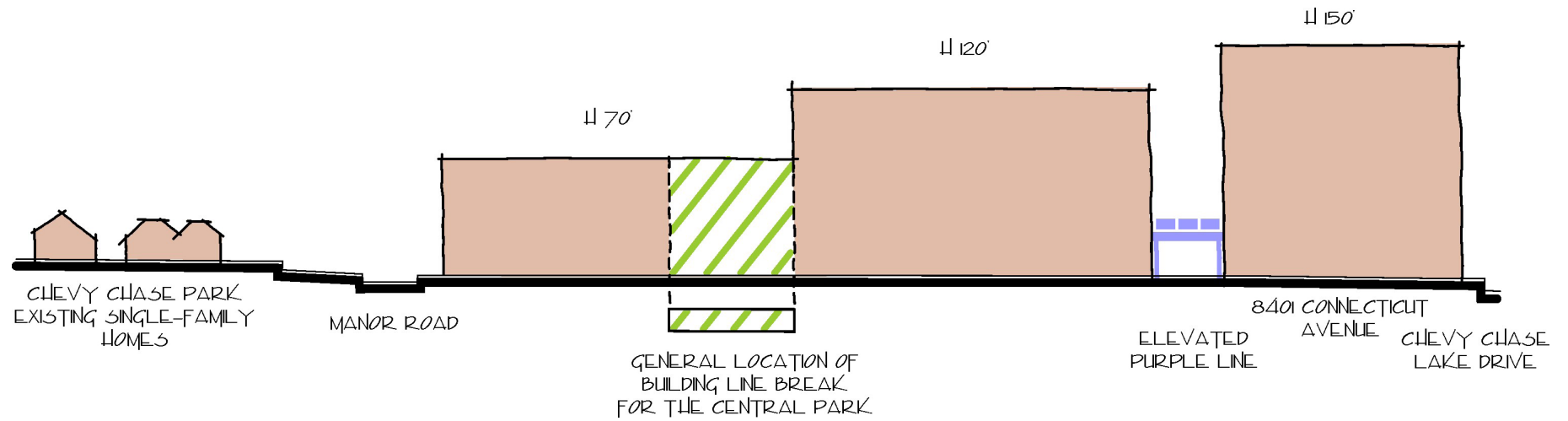
*Bridge crossings should be viewed as attractive landmarks, stressing the importance of color, materials, and appropriately scaled forms.*

#### Sketch 2 Purple Line Platform Sight Lines



*THE CENTRAL OPEN SPACE SHOULD INCLUDE LANDMARK VISIBLE FROM THE PLATFORM*

## Illustration 2 Elements Connecticut Avenue, Section



LOCATION MAP SHOWS EXISTING CONDITIONS



## Connecticut Avenue Boulevard

### Design Intent

The goal is to maintain Connecticut Avenue as a major highway, while transforming it into an attractive and pedestrian-friendly boulevard. Its design should contribute to a strong community identity and help support Chevy Chase Lake as a local destination. It should provide pleasant walking experiences with sidewalks that are buffered from traffic lanes. Pedestrian crossings should provide access to the Central Open Space and the Purple Line Station.

Short term, metered, on-street, parallel parking should serve restaurants and shops during off-peak hours. A two-way, shared use path, transitioning to a two-way cycle track between Chevy Chase Lake Drive and Manor Road, is planned along the east side of Connecticut Avenue to serve bicyclists desiring clear passage. Also along the east side, the sidewalk should be wider, in scale with the taller building heights. The right-of-way must also accommodate car-share spaces, taxi stops, bikeshare stations, bike racks, and bus stops. As properties redevelop, the number of driveways for access to Connecticut Avenue, and their widths, should be kept to a minimum to enhance walking routes.

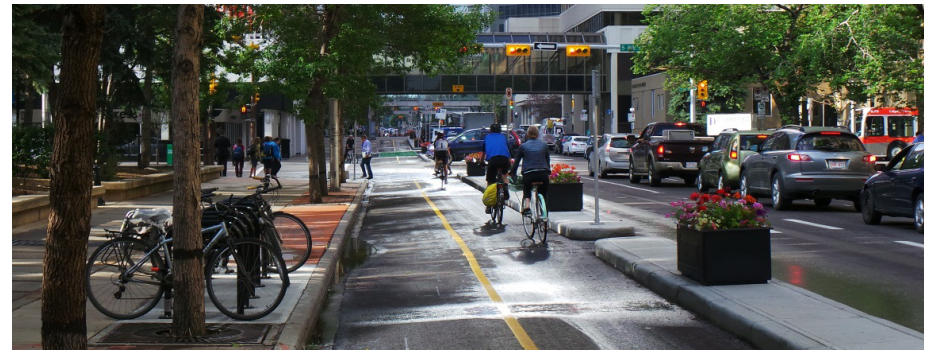
### Buildings

- Use building facades to create a continuous street wall along both sides of Connecticut Avenue and mark entry points into the Central Open Space.
- Design buildings with doors and windows from actively used interior spaces that overlook Connecticut Avenue and people walking under the Purple Line and Trail bridges.
- Step buildings back to maximize winter sun reaching sidewalks, parks, and the Purple Line station platform.

### Relationship to Central Open Space

- Break the continuous street wall to allow the Central Open Space to have a presence on Connecticut Avenue.
- Mark the edge of the park along Connecticut Avenue with street trees, landscaping, and furniture.
- The Connecticut Avenue streetscape should be coordinated with the design of the Central Open Space, especially at park entrances and transition areas.
- The design and placement of the Central Open Space should be coordinated with any new pedestrian crossings of Connecticut Avenue, as approved by MCDOT and SHA.

*Connecticut Avenue is classified as a major highway (M-7 on the Master Plan of Highways) with six travel lanes and a center median, and an expected speed limit of 35 miles per hour. The most representative cross section is MCDOT Design Standard 2008.01. All future improvements and design features will require MCDOT and SHA approval. While Connecticut Avenue is not anticipated as a bus rapid transit (BRT) route, it does accommodate regular bus service.*



*A two-way cycle track on the east side of Connecticut Avenue will have an island separating cyclists from traffic.*

### Street Edges

- The area between the building face and curb should provide adequate space for pedestrians and
  - street furniture, including:
  - street trees in landscaped tree panels
  - street lights
  - consolidated street signs
  - taxi and bus stops
  - a paved curbside edge with seating
  - display space for businesses
  - bike racks
  - a two-way cycle track on the east side
  - a bikeshare station.
- Street furnishings must be approved by MCDOT and SHA, and should reflect a design character that is unique to Chevy Chase Lake.

### Pedestrian Crossings

- Explore ways to shorten pedestrian crossings on Connecticut Avenue, cross streets and drive ways.
- Provide attractive landscaping within the median, including shade trees and pedestrian refuges.
- Improve pedestrian safety at the Connecticut Avenue/Laird Place intersection and make it accessible for all users. Providing a traffic signal to control this crossing is highly desirable and the signal warrants for this location should be studied in conjunction with future development.

### Outside the Center

- Maintain design elements that define Connecticut Avenue as a single, unified street, but with a more urban character within the Center and a less urban one outside it.



*Arches, light bollards, and paving designs can enhance pedestrian crossings.*



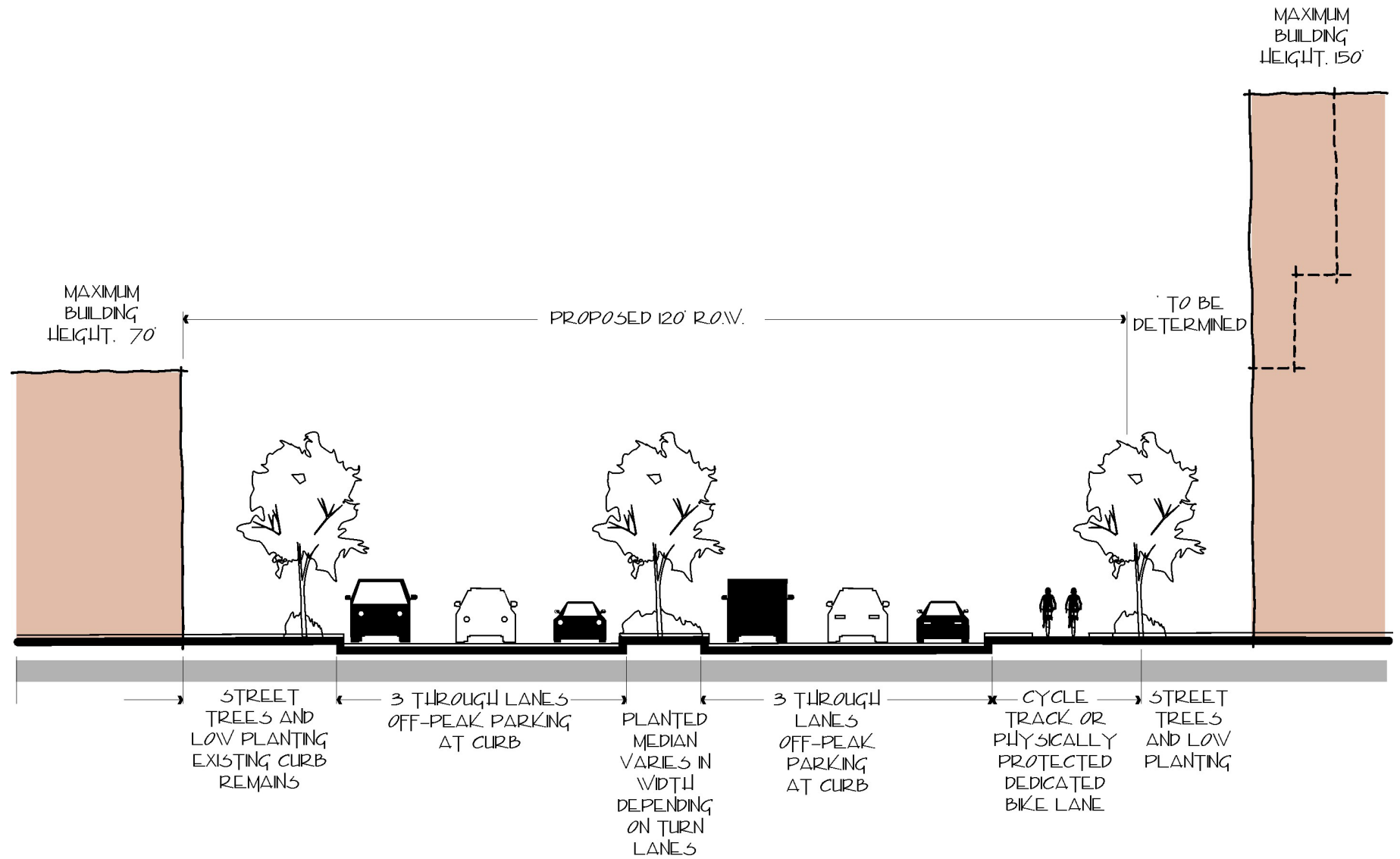
*The character of Connecticut Avenue changes both north and south of the center*





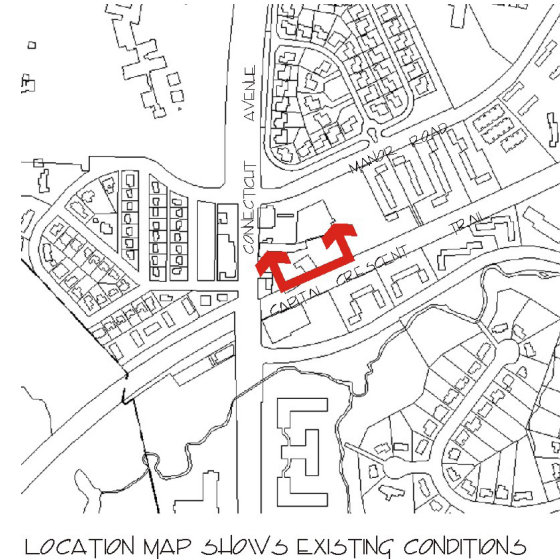
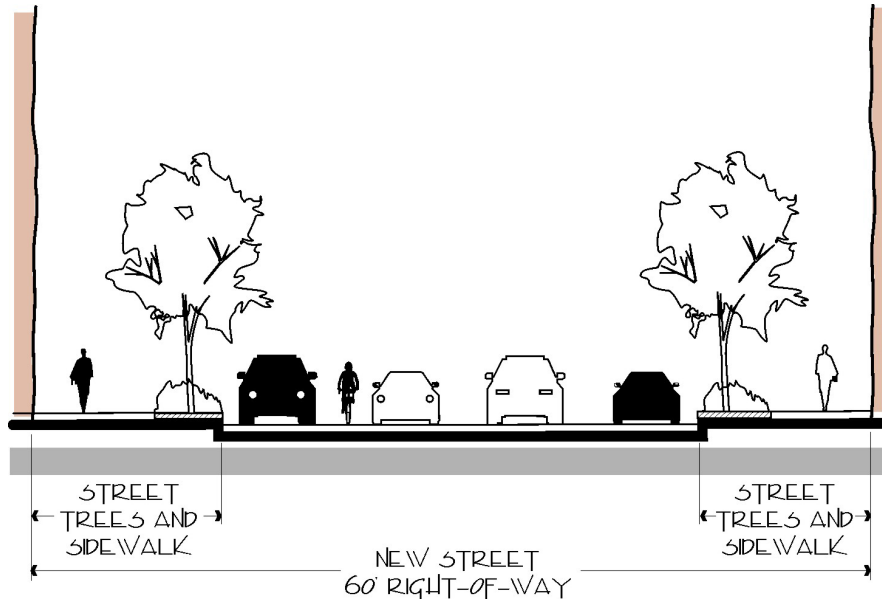
*A boulevard cross section can help make a more pleasant and safe crossing for pedestrians on Connecticut Avenue.*

### Illustration 3 Connecticut Avenue Boulevard





#### Illustration 4 New Street Detailed, Section



### New Street

#### Design Intent

New Street is an important north-south visual and physical connection within the Center under the elevated Purple Line. It should be designed as a unified environment that accommodates a vehicular, bicycle and pedestrian link.

The Plan recommends New Street as a business district street that uses the Design Standard 2005.01 cross section. It is envisioned with two travel lanes, on-street parallel parking serving street-fronting retail, and a target speed of 25 miles-per-hour. It will accommodate cyclists on a signed shared roadway and help support bikeshare stations.

#### Buildings

- Use building facades to create a continuous street wall on both sides and a well-defined public realm.
- Design buildings with doors and windows that overlook the street to provide views of people approaching and under the Purple Line and Trail bridge.
- Orient buildings to minimize shading outdoor public areas and spaces, especially during winter months.

### Street Edge

- The area between the building face and curb should accommodate:
  - street trees and landscaping or trees within grates
  - street lights
  - consolidated street signs
  - taxi and bus stops
  - a paved curbside edge
  - seating
  - display space for businesses
  - bike racks
  - bikeshare station.

### Public Space

- Where buildings line only one side of New Street, for example opposite the Central Open Space, trees, landscaping, and street furnishings should be used to define the street edge.
- The street design should be flexible, allowing it to be used as an extension of the Central Open Space.

### The Purple Line Underpass and the Center

- Use consistent streetscaping along the street's length.
- The passage beneath the Purple Line should be well lit and inviting. It should have clear sight lines from the north and south entrances and from across the New Street.

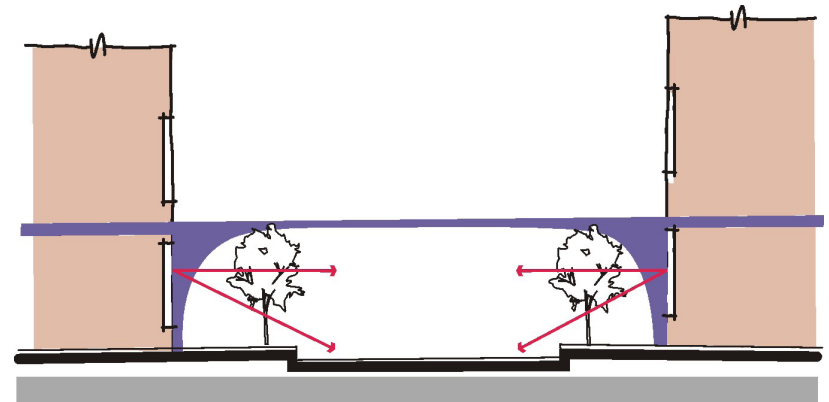
### Underpass Visibility

- The underpass should:
- be as wide and spacious as possible to accommodate two travel lanes shared by motorists and bicyclists
- have sidewalks on both sides that are set back from the curb with an offset from the underpass walls
- include parallel on-street parking on both sides
- include a protective barrier for pedestrians, such as bollards, rails, or a low wall
- should maintain openness underneath and in its relationship with adjacent buildings and open spaces
- have light and reflective surfaces
- include public art
- apply CPTED principles.

*An overhead structure can help create a sense of enclosure.*



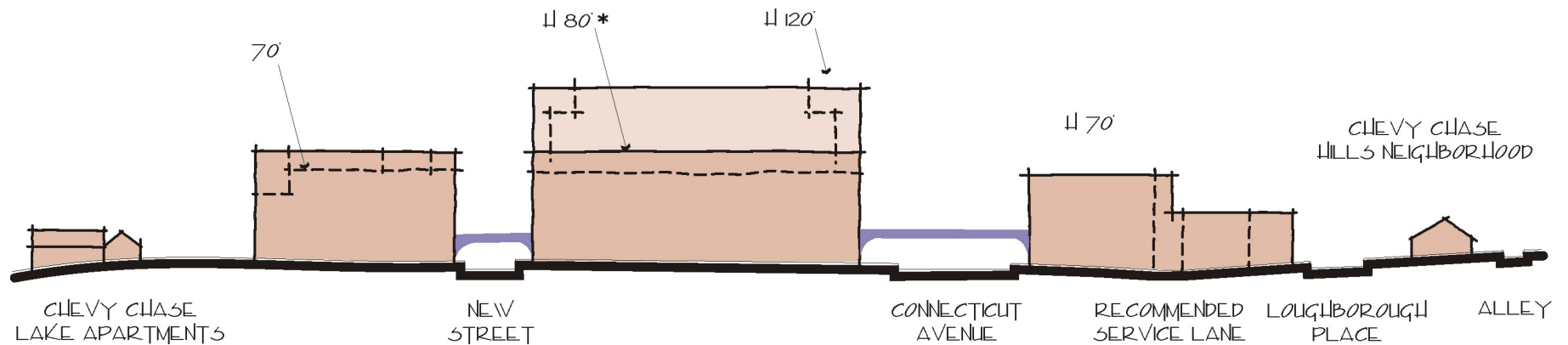
### Sketch 3 Underpass Visibility



MAXIMIZE THE CLEAR SPACE UNDER BRIDGE TO CREATE A PLEASANT WALKING ENVIRONMENT WITH LIGHT, AIR, AND CLEAR SIGHT LINES.



### Illustration 5 Chevy Chase Lake Shopping Center Section



\* BUILDING HEIGHT OF 80' IS ONLY ALLOWED FOR A HOTEL. OTHERWISE, 70' IS THE MAXIMUM HEIGHT ALLOWED. THE LOCATION AND DEPTH OF BUILDING STEPBACKS WILL BE DETERMINED AT THE TIME OF DEVELOPMENT REVIEW. THE STEPBACKS SHOWN HERE BY DASHED LINE ARE ILLUSTRATIVE.

### Center

#### Chevy Chase Lake East Shopping Center

##### Design Intent

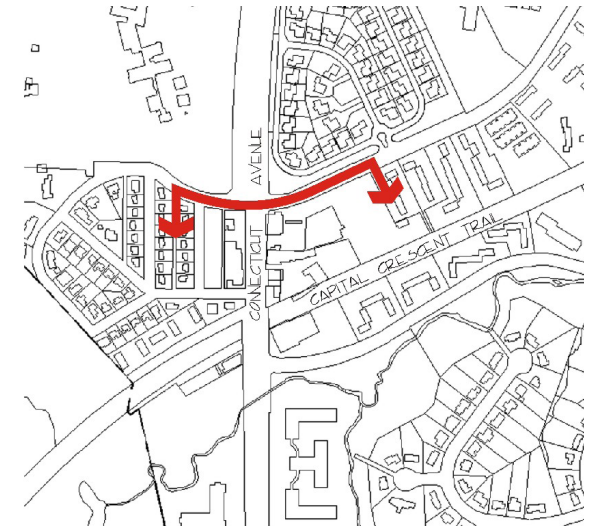
Allow redevelopment with mixed uses, street-oriented retail, and a central open space to transform the Chevy Chase Lake Shopping Center to become the heart of a broader Chevy Chase Lake community, compatible with its overall character and identity.

##### Buildings

- Create a continuous building line with entrances and windows along streets.
- Orient storefronts to face Connecticut Avenue and the streets bordering the Central Open Space.
- Locate the tallest buildings near the Purple Line station.
- Design buildings to reduce their impacts on views, shade, and privacy; particularly as they relate to existing neighborhoods.

##### Streets

- Blocks should be short and walkable.
- Service drives and loading areas should minimize conflicts with pedestrians and vehicles. Loading and waste storage areas should be located away from public areas and hidden from view by gated screening enclosures.



LOCATION MAP SHOWS EXISTING CONDITIONS

### The Central Open Space

- Building location and orientation should ensure that sun reaches the central open space throughout the year.
- Outdoor spaces should be accessible, visible, and flexible, accommodating large and small gatherings, special events, and for individual enjoyment.
- Design should consider a central lawn area of about one half acre.
- The park should be bordered by activating uses except where the park meets Connecticut Avenue.
- Consider a design that arranges one full block of the Central Open Space to seamlessly meet the adjacent building faces without an intervening street.
- Anchor the park with a landmark that is visible from the Purple Line station and the west side of Connecticut Avenue.

### Adjacent Neighborhoods

- Facing Manor Road, land uses, building design, and landscaping should transition to the existing single-family neighborhood.
- Attractive views of new development should be created through landscaping, setbacks, and site and building design.
- Facing the Chevy Chase Lake Apartments, create a compatible transition by:
  - stepping down building heights from the allowed maximum to heights that are compatible with the existing two and one half story apartment buildings.
  - increasing building setbacks commensurate with increased building heights
  - orienting new development to share its open space with the residents of the existing apartment buildings.
  - designing new buildings to reflect the scale, rhythm, and patterns established by the existing apartment buildings
  - protecting existing, mature trees along shared property lines
  - keeping service and maintenance areas, functions, and access away from the fronts of existing apartment buildings.

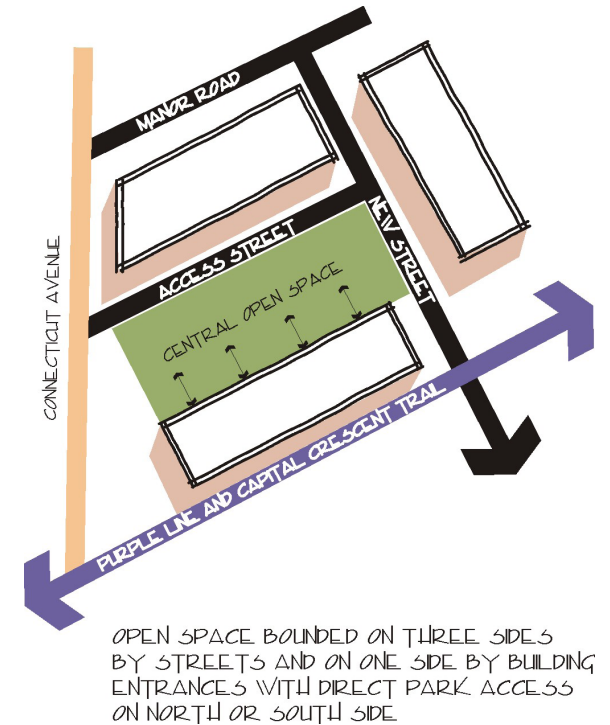
### Gateway Corners

- Mark gateway entrances to the Center with building and site design. The elements, design features, and materials of other Center development and structures, such as the bridges over Connecticut Avenue, should be well-coordinated.
- Ensure that the design of Connecticut Avenue is compatible with the design of Manor Road.

### Parking

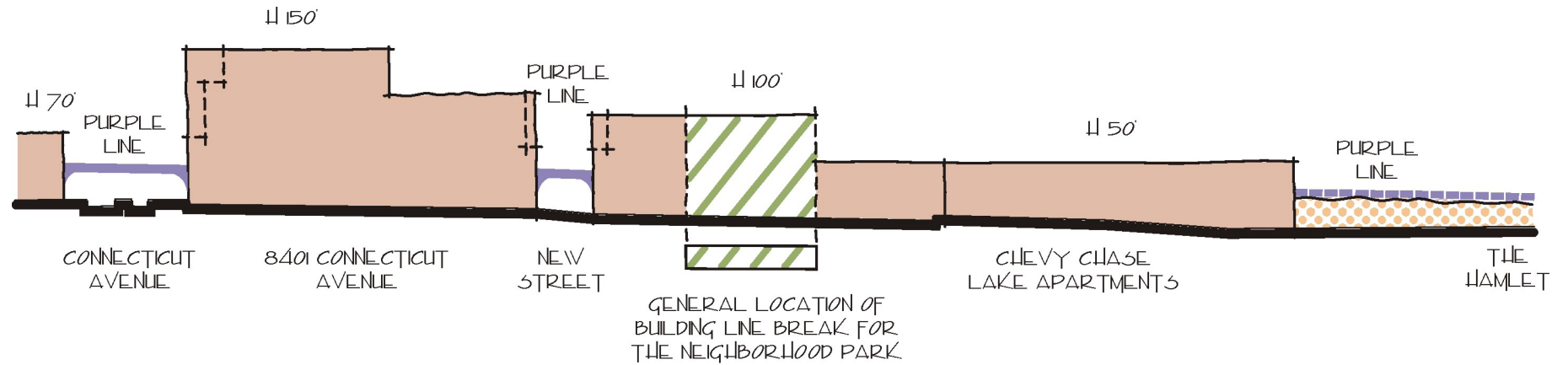
- The location and design of parking areas should:
  - maximize on-street, parallel parking within the parameters set by MCDOT and SHA place off-street parking out of sight, either underground or hidden by buildings avoid or screen views of parked cars and headlights from nearby homes.

### Sketch 4 Central Open Space

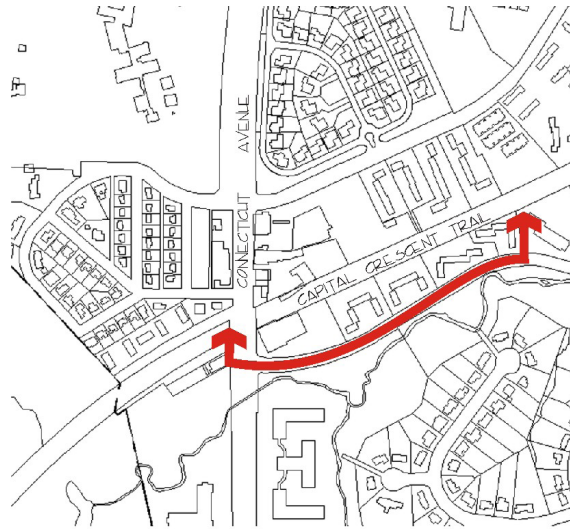




## Illustration 6 Chevy Chase Lake Apartments, Section



THE DEPTH AND LOCATION OF BUILDING STEPBACKS WILL BE DETERMINED AT DEVELOPMENT REVIEW. THE STEPBACKS SHOWN HERE BY DASHED LINES ARE ILLUSTRATIVE.



LOCATION MAP SHOWS EXISTING CONDITIONS

- Parking structures that are visible from other properties or public spaces should blend with
  - surroundings by:
  - screening and buffering with trees and landscaping
  - using green walls and ornamental screens
  - placing active uses at ground level
  - designing parking structures to look like residential or commercial buildings.

## Chevy Chase Lake Apartments

### Design Intent

The Plan calls for redevelopment of this site to create additional affordable housing in the community. New development should transition between Connecticut Avenue and the existing townhouse communities and should include a new public space that is a destination for the entire community.

### Recreation Features

- The park “should be designed per Parks Department standards” and consider including seating, picnic areas, and play equipment.
- The park design should consider the surrounding residential uses, for example, the noisiest park activities should be located away from private living spaces.
- CPTED principles should be followed, in particular to provide clear views to the park from Chevy Chase Lake Drive.

### Connectivity

- Good pedestrian connections should be provided across Chevy Chase Lake Drive between the park and the Tranquility Trail.
- The park should be visible and accessible from the proposed Walking Loop.

### Transitions

The Sector Plan proposes new residential development here and recommends limiting building height on the site’s eastern end to 50 feet.

- New development should provide a transition between taller and denser development near New Street and the Purple Line Station, and the existing Hamlet House condominiums.
- Landscaping at the shared property line should create a soft transition, taking into consideration the views from the balconies and interior spaces of the Hamlet House condominiums.



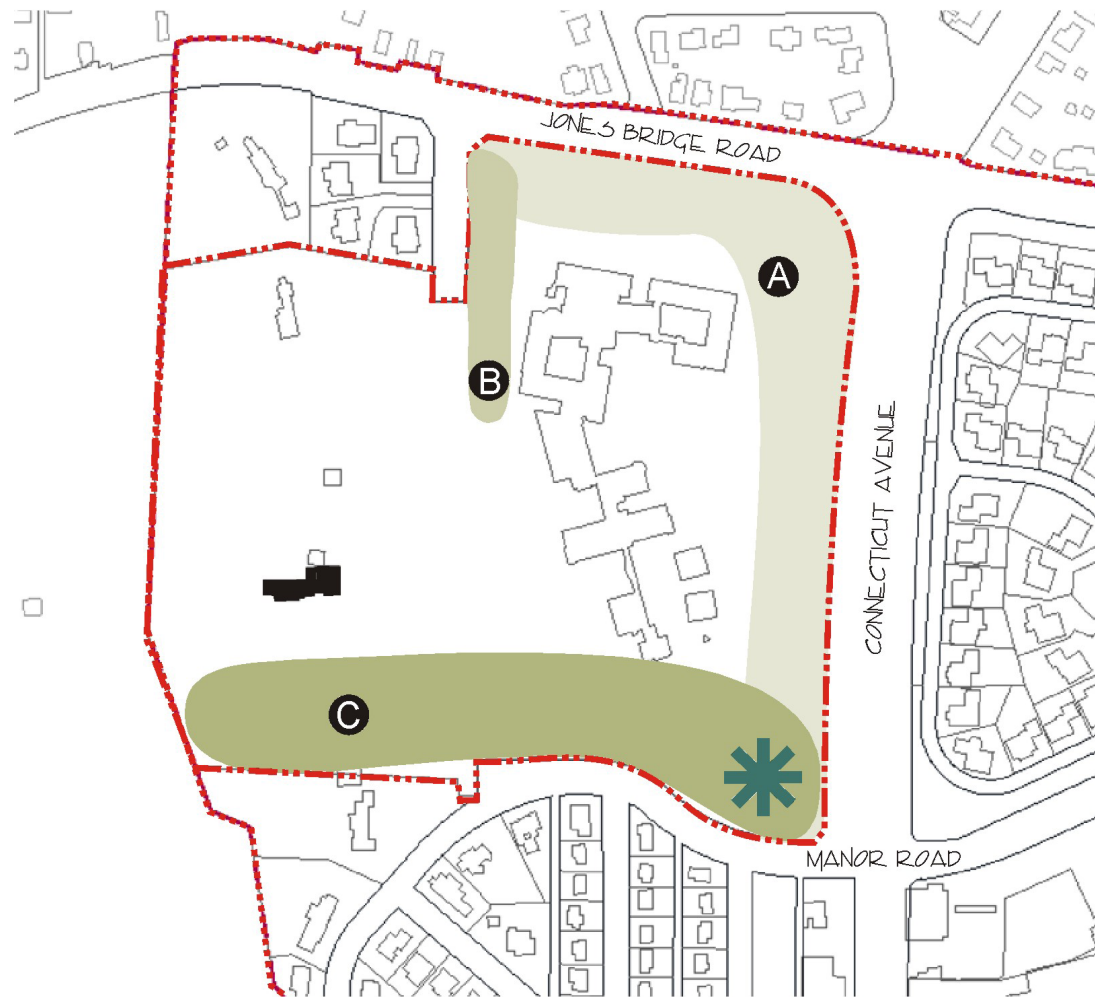
*Parking structures should have retail facing the street, and look more like offices or apartments.*










*The new neighborhood park could include a playground.*



## Map 5 Howard Hughes Medical Institute



-  GENERAL AREA FOR NEIGHBORHOOD OPEN SPACE
-  WOODLAND ALONG JONES BRIDGE ROAD AND CONNECTICUT AVENUE
-  LANDSCAPING AT GUARD HOUSE ENTRY ON PLATT DRIVE
-  LANDSCAPING WITH EXPANSIVE LAWN AND MATURE TREES
-  HISTORIC HAYES MANOR
-  PROPERTY BOUNDARY
-  PLAN BOUNDARY



NOT TO SCALE

## Other Sites

### Howard Hughes Medical Institute

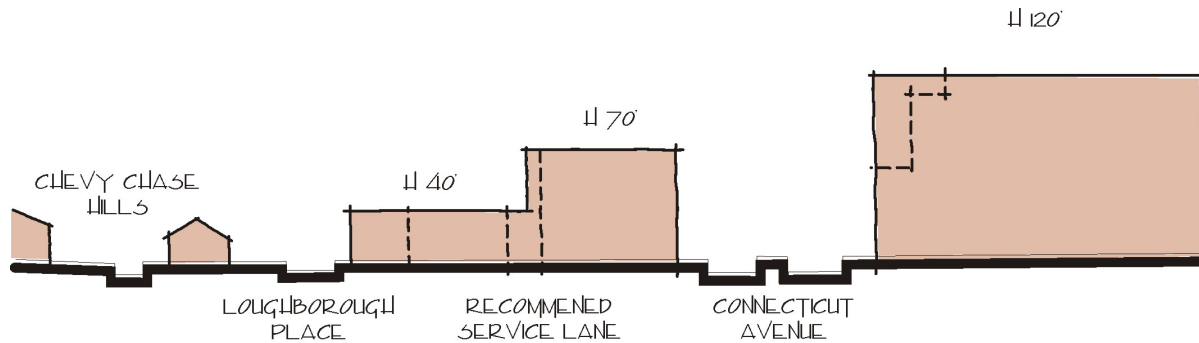
#### Design Intent

The Plan relies on these guidelines to address compatibility of any new development on the Howard Hughes Medical Institute (HHMI) campus that is visible from the adjacent Chevy Chase Hills community and from Platte Road. Compatibility is best addressed by preserving the character of the campus—its wooded setting and low-scale, buildings. The presence of HHMI should be reinforced through compatible site features prominently featured in a location near the Center and Manor Road.

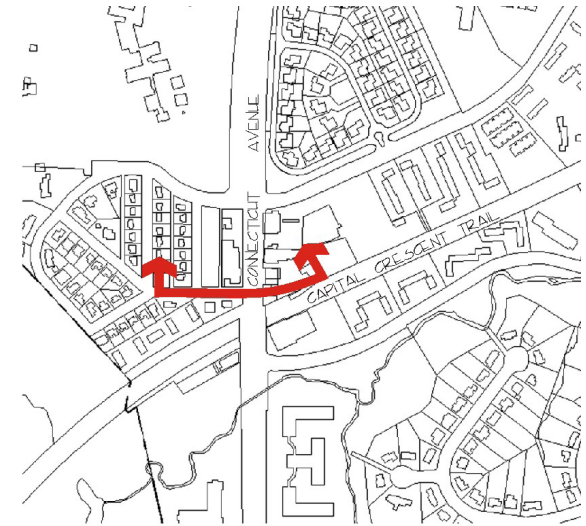
- Maintain the current campus character including:
  - the woodland along Jones Bridge Road and Connecticut Avenue
  - the landscaping at the guard house entry on Platt Drive
  - the landscaping and expansive lawn with mature trees along Manor Road, which provide a transition to the adjacent Chevy Chase Hills neighborhood.
- Future Neighborhood Public Space  
Expansion of HHMI will require approvals by public agencies, at which time opportunities to provide significant public use space and amenities should be explored. That development review should further consider:
  - a neighborhood public open space along Manor Road that provides recreation for the broader community
  - using artwork, or fencing and walls that include images and descriptions of the institute's work and mission.



**Illustration 7** Chevy Chase Lake West Shopping Center, from Laird Place

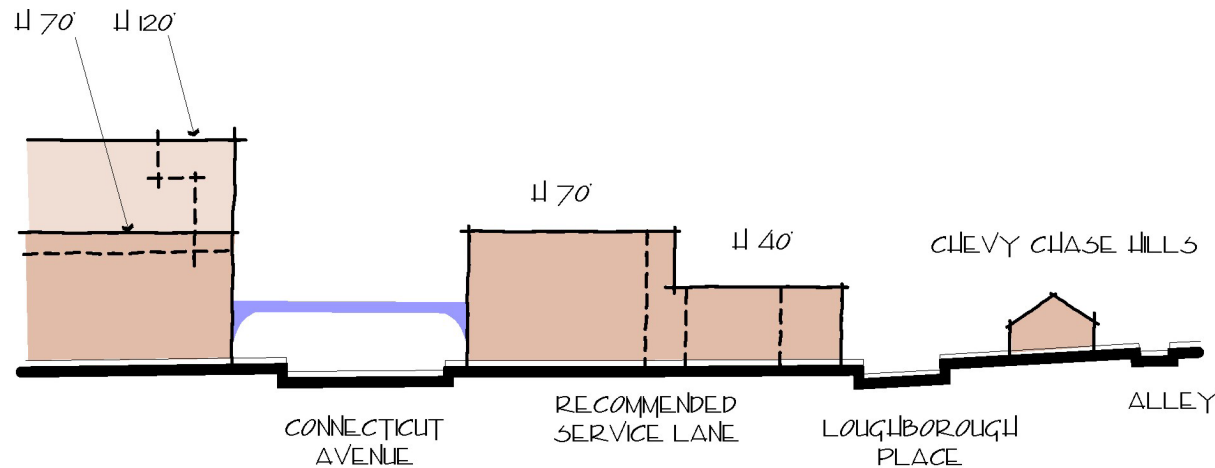


THE DEPTH AND LOCATION OF BUILDING STEPBACKS WILL BE DETERMINED AT DEVELOPMENT REVIEW. THE STEPBACKS SHOWN HERE BY DASHED LINES ARE ILLUSTRATIVE.

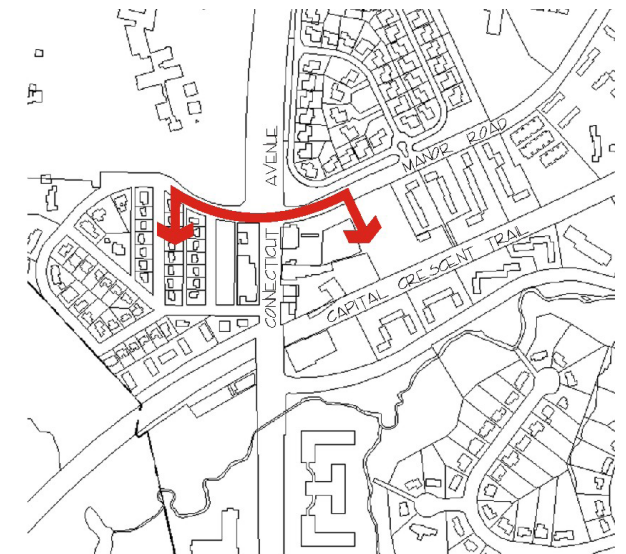


LOCATION MAP SHOWS EXISTING CONDITIONS

**Illustration 8** Chevy Chase Lake West Shopping Center, from Manor Road



THE DEPTH AND LOCATION OF BUILDING STEPBACKS WILL BE DETERMINED AT DEVELOPMENT REVIEW. THE STEPBACKS SHOWN HERE BY DASHED LINES ARE ILLUSTRATIVE.



LOCATION MAP SHOWS EXISTING CONDITIONS

## Chevy Chase Lake West Shopping Center and Loughborough Place

### Design Intent

This is a gateway to the Center and an entrance to an established neighborhood. Its design should be coordinated and consistent with development on the east side of Connecticut Avenue. New residential development should blend with the character of the Chevy Chase Hills neighborhood and enhance Loughborough Place as a local street.

- Apply the design guidelines for Connecticut Avenue (page 27).
- Ensure a strong physical and visual connection across Connecticut Avenue by:
  - maintaining key sight lines to landmarks on the east side, such as the Central Park
  - providing a through-block pedestrian connection between Loughborough Place and Connecticut Avenue, in coordination with a pedestrian crossing of Connecticut Avenue
  - designing buildings to complement the buildings and the Central Park on the east side of Connecticut Avenue.

Along the first block of Laird Place, development should transition to the Chevy Chase Hills neighborhood by stepping down building heights and setback buildings. Development should also enhance pedestrian crossings of the alley and include street trees.

Along the first block of Manor Road, development should transition and from Connecticut Avenue to Chevy Chase Hills and complement the existing neighborhood by stepping down building heights and providing ample setbacks to accommodate a sidewalk and a tree lawn with shade trees.

### Street Edge

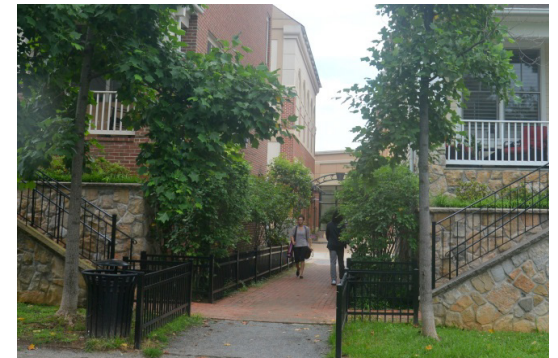
- Use landscaping that can establish quickly and mature to blend with the existing neighborhood.
- Provide a sidewalk, street trees, landscaping, seating, and street lights.
- Setbacks for new buildings should equal the existing front yards across the street, but may be reduced when accommodating a shared rear private alley.
- Since existing single-family homes do not have driveways and rely on rear alleys, driveways and alleys for new buildings should not directly access Loughborough Place. If unavoidable, then headlights should be screened from view of existing homes.



*New townhouses should be compatible with existing homes across the street.*

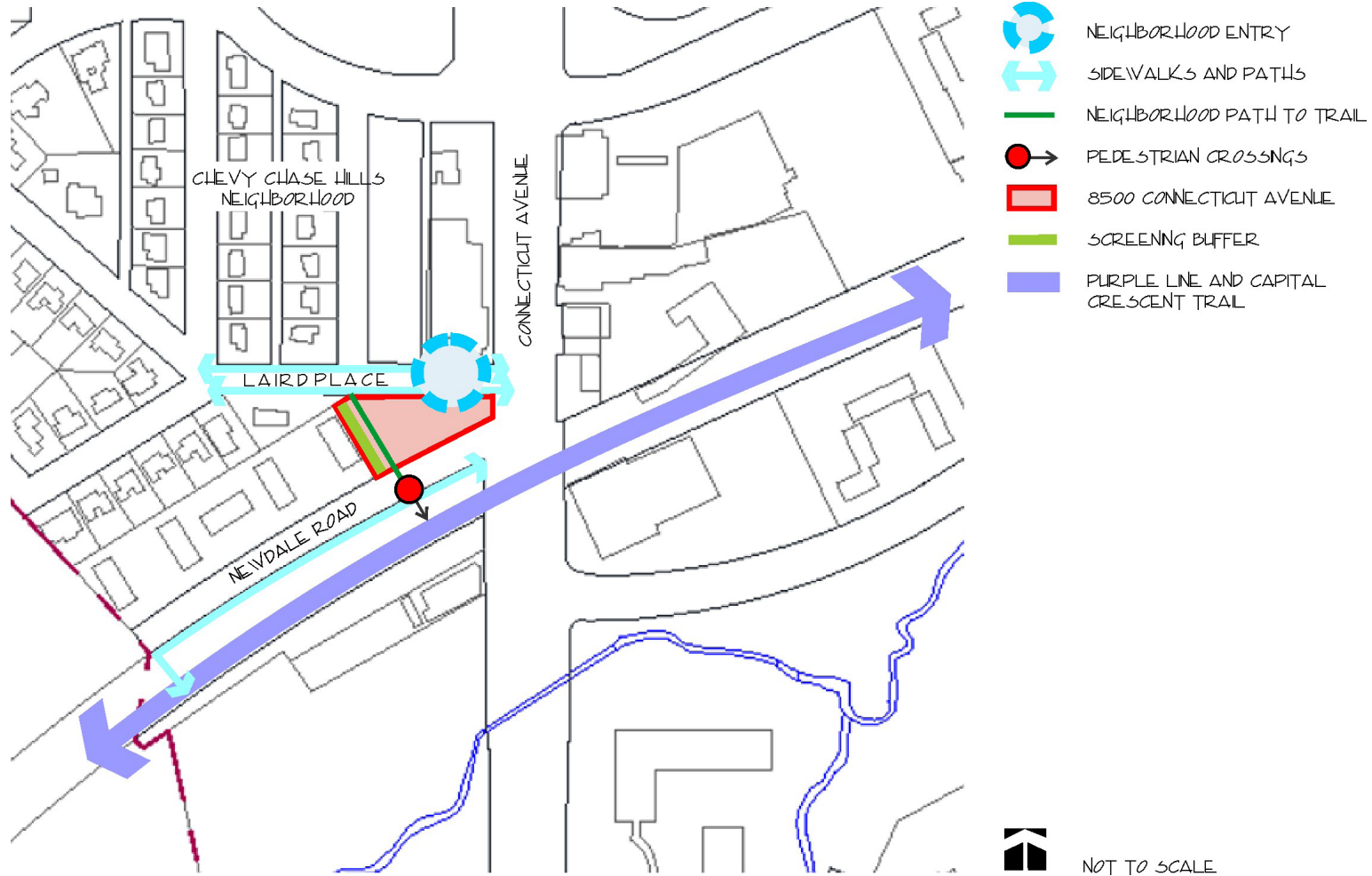


*New townhouses could share an alley with mixed use development planned on Connecticut Avenue.*



*A path between townhouses should connect to Connecticut Avenue.*

## Map 6 8500 Connecticut Avenue





## Buildings

- New buildings should be designed as short rows of townhouses, reflecting or reinterpreting the proportions, rhythms, and materials of existing, facing single-family detached homes.

### Through-Block Passages: Pedestrian Connections and Shared Private Alley

- New residential development, parking, and service areas should be accessed from a private alley that is shared with new mixed-use development along Connecticut Avenue.
- The public through-block pedestrian connection between Loughborough Place and Connecticut Avenue should:
  - be a direct mid-block route
  - be visible, well lit, and wide enough for people walking bikes to pass
  - be positioned with sight lines to engaging focal points
  - include design features compatible with the Central Park.

## 8500 Connecticut Avenue (Arman's Chevy Chase Service Station)

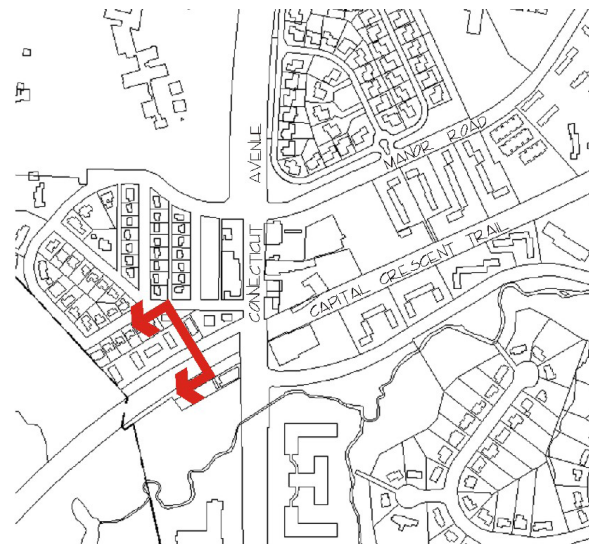
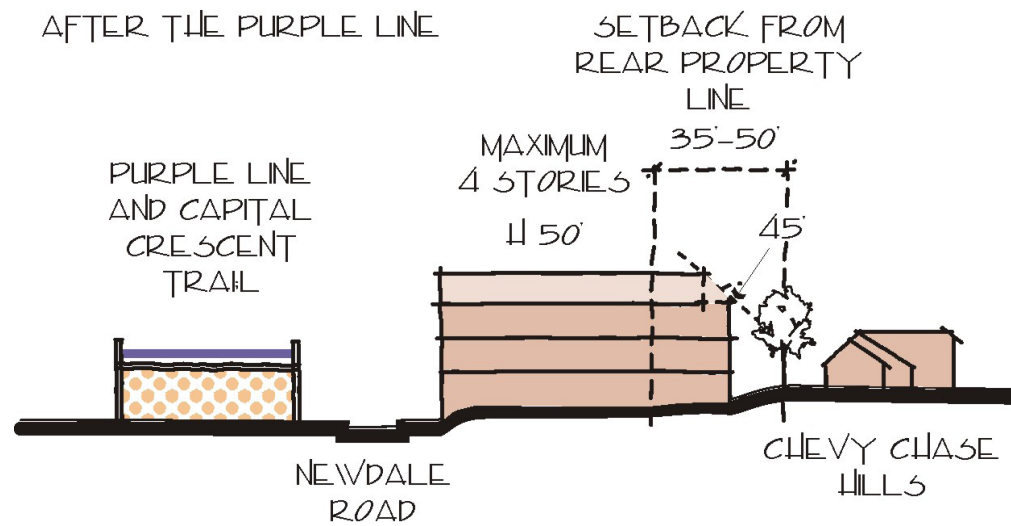
### Design Intent

This site on Connecticut Avenue between Newdale Road and Laird Place should create an entry into the Newdale Mews and Chevy Chase Hills neighborhoods. It should also accommodate direct passage from these neighborhoods to the Capital Crescent Trail at the end of Newdale Road.

### Relationship to Purple Line and Capital Crescent Trail, Bridges, and Underpass

- Apply the design guidelines for Connecticut Avenue.
  - Provide a path through the site, beginning at Laird Place and crossing Newdale Road, to the Capital Crescent Trail that minimizes street and driveway crossings. The path should connect directly to sidewalks or paths at either end and be wide enough for people to pass one another, whether jogging, walking, or biking.
  - The property will be highly visible from the elevated section of the Capital Crescent Trail and Purple Line so attention to design details must focus not only on areas visible from the ground, but also as seen from above.
  - Screen mechanical equipment.
  - Position doors and windows to provide views into the underpasses for surveillance and safety.
- ### Relationship to Newdale Mews and the Chevy Chase Lake West Shopping Center
- Any redevelopment should provide additional screening from Newdale Mews. The buffer's width may need to exceed the CRT Zone's required minimum setback to achieve compatibility.
  - If included as part of Newdale Mews or Shopping Center redevelopment, the design guidelines for those properties also apply.

## Illustration 9 Newdale Mews, Section



LOCATION MAP SHOWS EXISTING CONDITIONS



Single family homes with yards that back up to Newdale Mews.

## Newdale Mews

### Design Intent

The Plan identifies residential redevelopment compatible with Chevy Chase Hills as the most appropriate use.

### Building Form and Location

- Maximum building height is 50 feet, measured from Newdale Road, and no building should be more than four stories tall.
- New development should be set back at a distance providing an adequate buffer for adjacent single-family homes. The Plan recommends “new development should be set back from the rear property line between 35 and 50 feet,” which may be greater than the minimum required in the CRT Zone. Likewise, a sufficient distance must also be maintained between new buildings and the Purple Line.
- Rather than a single, continuous building, create several shorter ones that reference the architectural forms, rooflines, materials, design details, and proportions of neighboring homes.
- To maximize views for neighboring homes, arrange new buildings with intervening open spaces that open to Newdale Road.
- Such open space should be designed to provide attractive views through the site for neighbors.

### Green Buffer

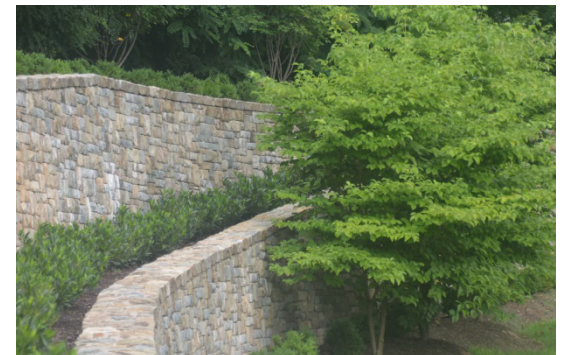
- Based on an arborist’s recommendations, retain existing, healthy trees within the buffer.
- The buffer should be located on uncompacted soil where possible. A rooftop location, such as on a parking deck or within enclosed planters should be avoided. When unavoidable, minimize the extent to which the buffer encroaches onto a structure and minimize the use of planters. Soil depth should meet Forest Conservation Plan standards.
- Use plants suited to local site conditions.
- Use low walls or terraces to accommodate grade changes between the Newdale Mews property and adjacent homes.
- Use fences or trellises and intervening landscaping to screen adjacent homes.
- Consult with adjacent neighbors when developing a landscaping plan, paying particular attention to providing some winter light and strategically placing taller evergreens to enhance longer distance views.



*An attractive buffer between single family homes on the left, and apartments.*



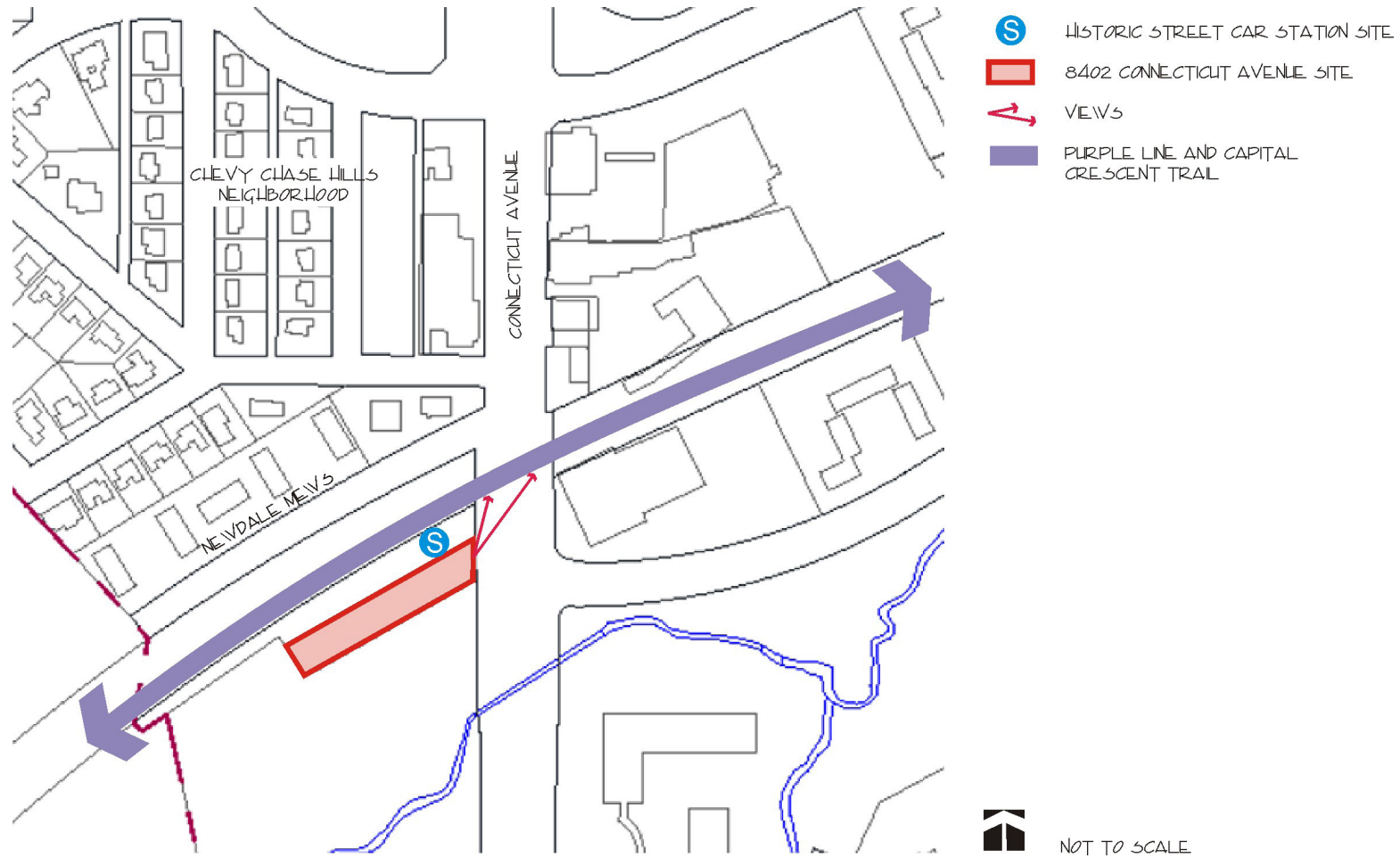
*Upper apartments step back, providing a good transition to single family homes.*



*Planted terraces can help screen views.*



## Map 7 8402 Connecticut Avenue



### North Building Faces

- Orient balconies, terraces, doors, and windows to avoid direct views into adjacent homes and backyards. When not possible, use architectural screens to minimize impacts.
- Exterior lighting adjacent to Chevy Chase Hills should be directed away from adjacent properties using low mounted, full sharp cut-off fixtures, at low illumination levels.
- Adjacent neighbors should be consulted when fine tuning design details and prior to construction to address concerns.

### During Construction

- Provide an attractive screen along the northern property line.
- Minimize glare and light trespass.

### 8402 Connecticut Avenue—Parkway Custom Dry Cleaning

#### Design Intent

This site marks an entrance into the Center from the west and south. Development should complement these entrances and mark the site of the streetcar station once located on the property.

#### Relationship to Purple Line and Capital Crescent Trail, Underpass, and Bridges

- The property will be highly visible from the elevated section of the Capital Crescent Trail and Purple Line so attention to design details must focus not only on areas visible from the ground, but also as seen from above.
- Screen mechanical equipment.
- Position doors and windows to provide views into the underpass to increase surveillance and safety.

#### Recognizing Local History

- Coordinate with local historic preservation groups to provide interpretive signs and markers for the Chevy Chase Lake streetcar station historic site (Master Plan resource #35/11).



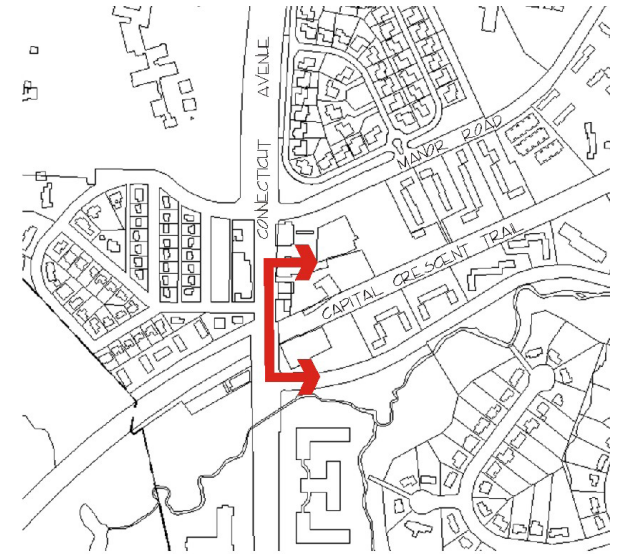
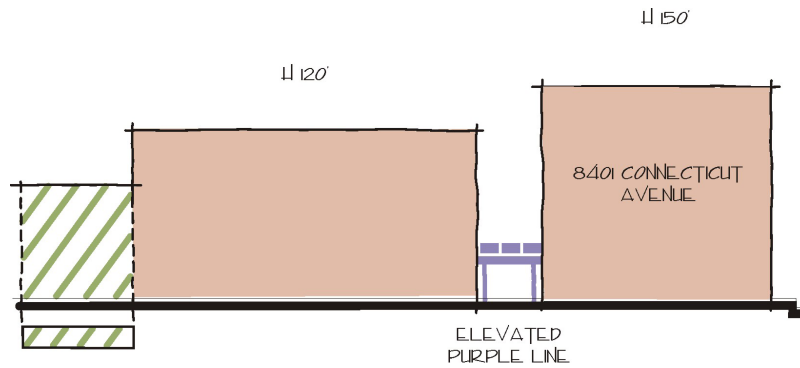
*Attractive, temporary fences should screen construction sites.*



*c. 1914 Chevy Chase Lake Station*  
Shows waiting men at Chevy Chase Lake where riders changed from their Capital Traction cars to the Kensington car. Summer car 813 has safety fence installed in front of car. The Kensington car stopped to the north of this station. The waiting room did not permit passengers a view of the Kensington Line; many travelers missed the ride home and waited an hour for the next car.  
PHOTOGRAPHED BY - WERNERLOUT SPENCER/STH

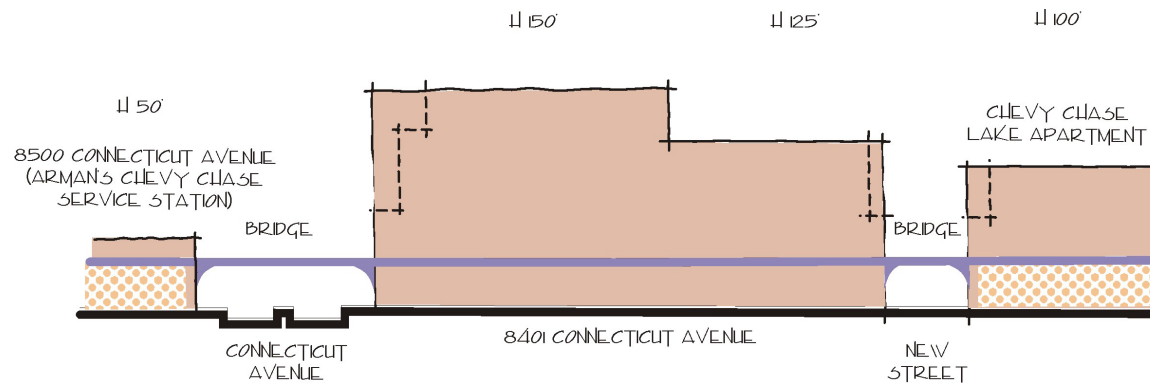
*Chevy Chase Lake Station c. 1914, was once located on this property.*

**Illustration 10 8401 Connecticut Avenue looking east**

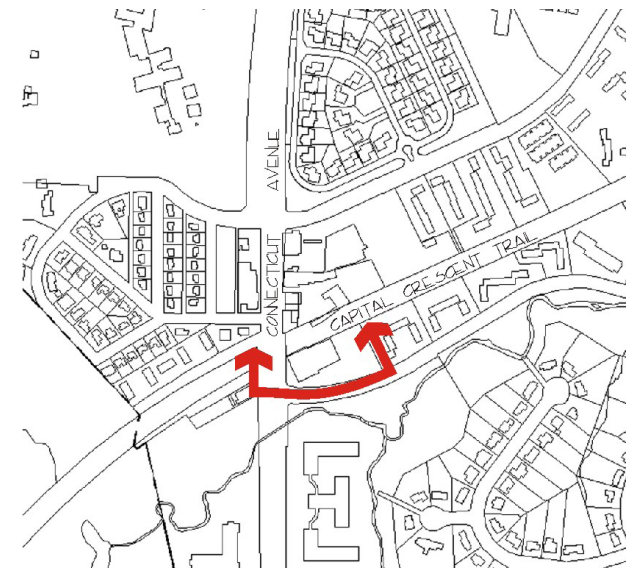


LOCATION MAP SHOWS EXISTING CONDITIONS

**8401 Connecticut Avenue Section looking north**



STEPBACKS, AS WELL AS THEIR LOCATIONS AND DEPTHS, WILL BE DETERMINED AT THE TIME OF DEVELOPMENT REVIEW. THE STEPBCKS SHOWN HERE BY DOTTED LINES ARE SYMBOLIC.



LOCATION MAP SHOWS EXISTING CONDITIONS



## 8401 Connecticut Avenue

### Design Intent

The current office building is the tallest in Chevy Chase Lake and the Plan envisions redeveloping it in a way that relates well to the new Center and the adjacent Purple Line and Trail. Redevelopment should fit without being visually dominating and create an appropriately scaled entrance to the residential communities along Chevy Chase Lake Drive. It should also contribute to an attractive and activating presence on the three surrounding streets: Connecticut Avenue, Chevy Chase Lake Drive, and New Street.

### Relationships to the Purple Line Station

- Building entrances should have direct access to the station platform.
- Building arrangements should avoid the creation of wind tunnels and maximize sunlight on the station platform.

### Relationship to Connecticut Avenue, Chevy Chase Lake Drive, and New Street

- Apply the design guidelines for Connecticut Avenue.
- Create a continuous building wall, with pedestrian-scaled details, ample windows, and street-level entrances on all three streets.

### Fitting In without Dominating

- Building forms should ensure good sight lines to the Center from the south and north and allow the Purple Line station to be easily seen.
- Buildings should complement, but not dominate street level views of the Purple Line and Capital Crescent Trail bridges over Connecticut Avenue, as seen from the north and the south.

### North Building Faces

- Orient balconies, terraces, doors, and windows to avoid direct views into adjacent homes and backyards. When not possible, use architectural screens to minimize impacts.
- Exterior lighting adjacment to Chevy Chase Hills should be directed away from adjacent properties using low mounted, full sharp cut-off fixtures, at low illumination levels.
- Adjacent neighbors should be consulted when fine tuning design details and prior to construction to address concerns.

### During Construction

- Provide an attractive screen along the northern property line.
- Minimize glare and light trespass.



*While 8401 Connecticut Avenue currently dominates the view, new development should blend in.*

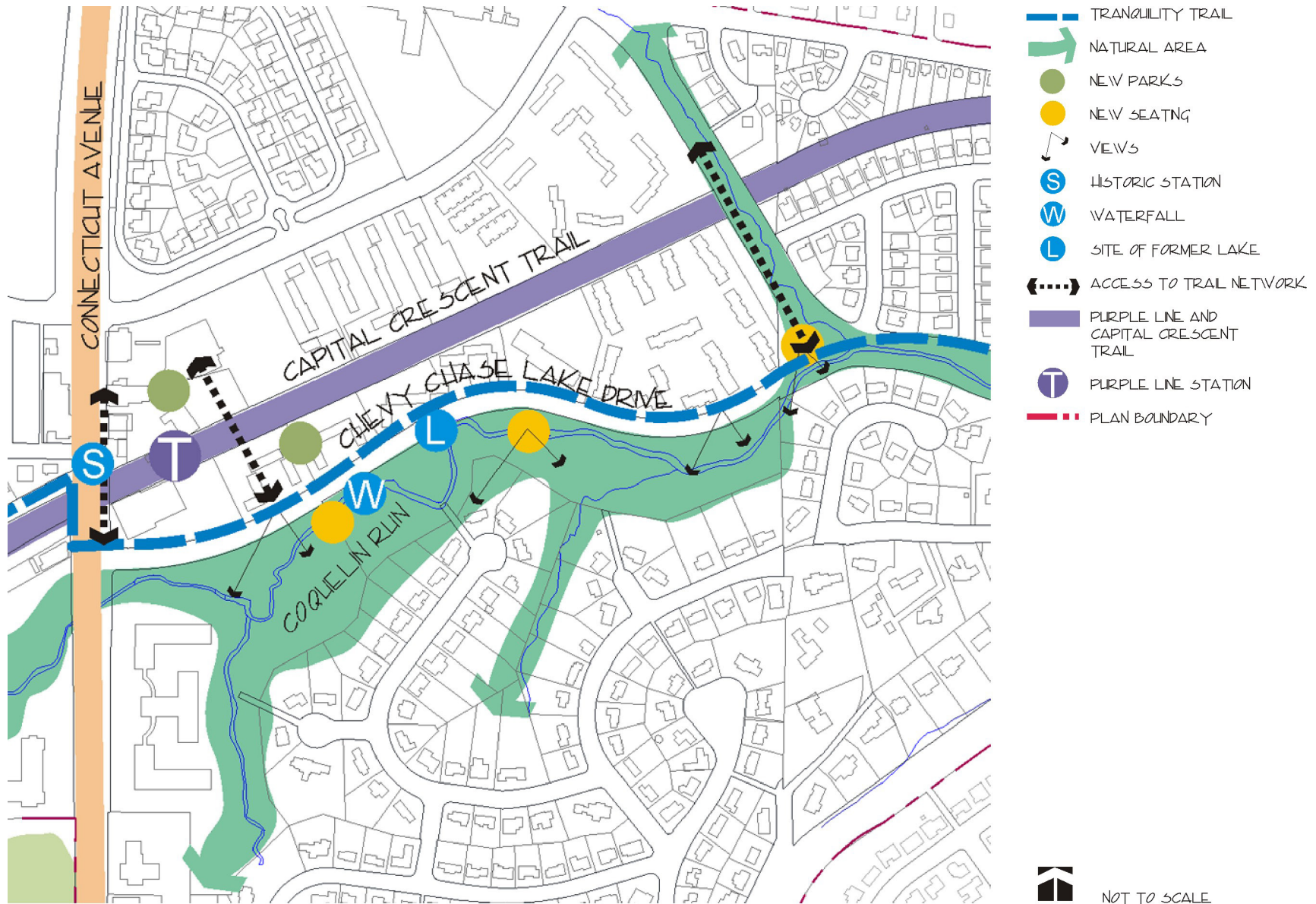


*Streets should be fronted by uses with ample windows and doors.*



*Active building and store fronts should line underpass approaches.*

## Map 8 Tranquility Trail





## Tranquility Trail

### Design Intent

The Tranquility Trail foot path will allow visitors to experience Coquelin Run—its wooded valley, waterfall, and wildlife.

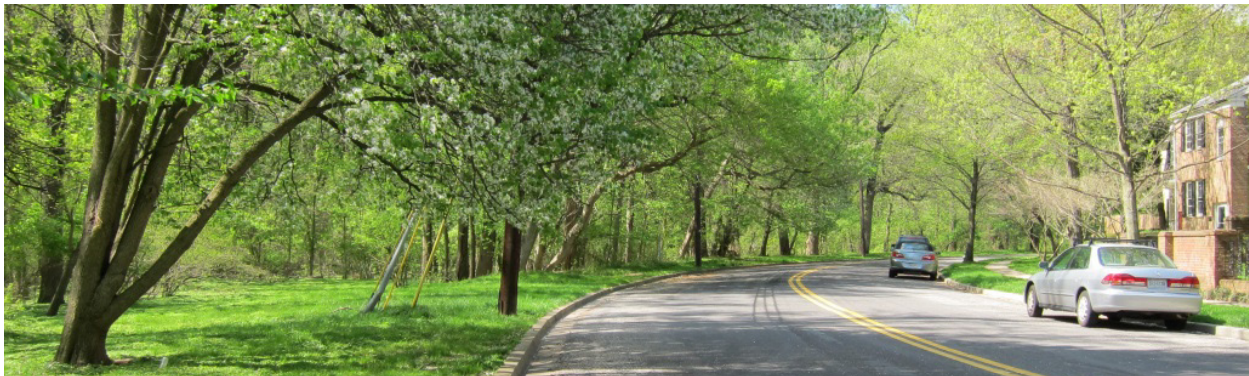
### Relationship to Chevy Chase Lake Drive

- The Plan recommends the trail within the right-of-way of Chevy Chase Lake Drive be owned by Montgomery County. Changes in its location may occur if permitted on adjacent private land.
- Coordinate the trail design and the street edge and provide a planted buffer, allowing adequate visibility and safety.
- Provide pedestrian crossings of Chevy Chase Lake Drive to the trail that meet MCDOT standards.

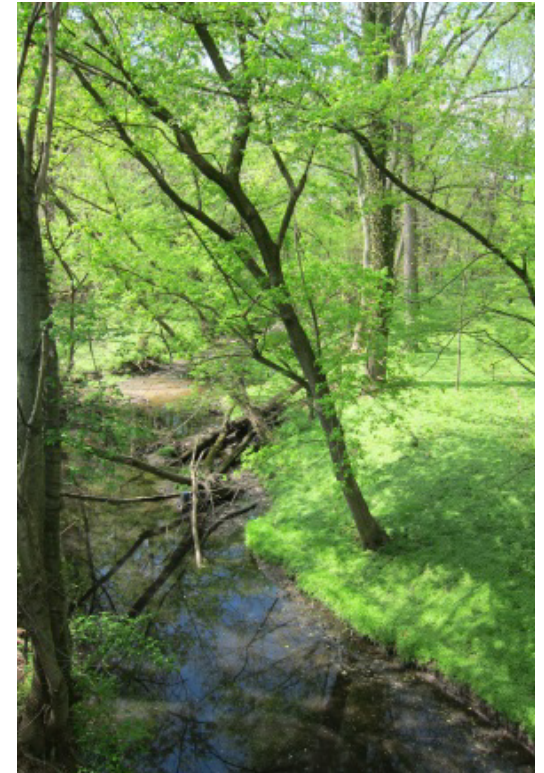
### Design and Construction

Minimize potential environmental impacts by:

- routing the trail to avoid tree roots and wildlife habitats and when possible, through areas already altered, such as stream restoration areas
- minimizing grading and other disturbance
- using pervious surfaces for trails and sitting areas
- applying innovative ADA compliant designs, which provide access while protecting the trail experience
- accommodating a small seating area among existing trees to view the waterfall and avoiding the removal of native canopy and understory trees, shrubs, and groundcover
- using seating and benches made of natural materials that reflect the natural stream valley character.



*The Tranquility Trail will be located along the south side of Chevy Chase Drive, along with a planted roadside buffer.*



*Coquelin Run*



*Coquelin Run*



## Resources

The following redevelopment resources are provided for informational purposes only and the list is not exhaustive.

### Maryland-National Capital Park and Planning Commission

- M-NCPPC Chevy Chase Lake Sector Plan (2013 Approved and Adopted)  
<http://www.montgomeryplanning.org/community/chevychaselake/index.shtm>
- M-NCPPC County Bikeways Functional Master Plan  
[http://www.montgomeryplanning.org/transportation/bikeways/A\\_A/contents.shtm](http://www.montgomeryplanning.org/transportation/bikeways/A_A/contents.shtm)
- M-NCPPC Development Manual  
[http://www.montgomeryplanning.org/development/development\\_manual/index.shtm](http://www.montgomeryplanning.org/development/development_manual/index.shtm)
- M-NCPPC Commercial Residential Zone Overview  
[http://www.montgomeryplanning.org/development/com\\_res\\_zones.shtm](http://www.montgomeryplanning.org/development/com_res_zones.shtm)
- M-NCPPC Commercial/Residential Zones Incentive Density Implementation Guidelines June 2012  
<http://www.montgomeryplanning.org/viewer.shtm#http://www.montgomeryplanning.org/development/documents/CRZoneGuidelinesFINAL.pdf>
- M-NCPPC 2012 Parks, Recreation and Open Space (PROS) Final Draft July 2012 9/12/12  
[http://www.montgomeryparks.org/PPSD/ParkPlanning/Projects/LPPRP/LPPRP\\_2005/2005\\_pros\\_lpprp\\_all.pdf](http://www.montgomeryparks.org/PPSD/ParkPlanning/Projects/LPPRP/LPPRP_2005/2005_pros_lpprp_all.pdf)
- M-NCPPC Purple Line Functional Master Plan  
[http://montgomeryplanning.com/transportation/projects/purple\\_line.shtm](http://montgomeryplanning.com/transportation/projects/purple_line.shtm)

## Montgomery County

- Montgomery County Zoning Ordinance - Chapter 59 [http://www.amlegal.com/montgomery\\_county\\_md/](http://www.amlegal.com/montgomery_county_md/) CRT Zone - Section 59-C-15.1
- Montgomery County Code  
[http://www.amlegal.com/montgomery\\_county\\_md/](http://www.amlegal.com/montgomery_county_md/)

Chapter 19 Erosions, Sediment Control and Stormwater Management

Chapter 22A Forest Conservation-Trees

Chapter 49 Streets and Roads

Chapter 50 Subdivision

- Department of Permitting Services Outdoor Café Seating Guide  
<http://permittingervices.montgomerycountymd.gov/permitting/pdf/OutdoorCafeSeating.pdf>
- Department of Permitting Services Sidewalk Vendor Operation and License [http://www.montgomerycountymd.gov/content/council/pdf/SCANNED\\_DOCS/20070227\\_16-61.pdf](http://www.montgomerycountymd.gov/content/council/pdf/SCANNED_DOCS/20070227_16-61.pdf)
- Department of Permitting Services Building Construction – Building Codes & Standards <http://permittingervices.montgomerycountymd.gov/dpstmpl.asp?url=/permitting/bc/nfbldc.asp>
- Department of Permitting Services Water Resources Documents including Stormwater Management  
<http://permittingervices.montgomerycountymd.gov/DPS/waterresource/WaterResourceDocuments.aspx>
- Montgomery County Maryland Historic Preservation Commission  
<http://www.montgomeryplanning.org/historic/>
- Department of Transportation Pedestrian Safety  
<http://www.montgomerycountymd.gov/dottmpl.asp?url=/Content/dot/dir/pedsafety/index.asp>
- Department of Transportation Design Standards (for Streets and Roads)  
<http://www2.montgomerycountymd.gov/DOT-DTE/Common/Standards.aspx>
- Montgomery County Road Code (with urban street standards)  
[http://www.montgomerycountymd.gov/content/council/pdf/SCANNED\\_DOCS/20070715\\_48-06.pdf](http://www.montgomerycountymd.gov/content/council/pdf/SCANNED_DOCS/20070715_48-06.pdf)

- Connecticut Avenue Pedestrian Road Safety Audit – 2011- [http://www.montgomerycountymd.gov/dot-pedsafety/resources/files/prsa\\_reports/connecticut\\_prsa\\_draft\\_08-08-12\\_public.pdf](http://www.montgomerycountymd.gov/dot-pedsafety/resources/files/prsa_reports/connecticut_prsa_draft_08-08-12_public.pdf)
- Context Sensitive Road Design Standards, including Standard 040.01 Stormwater Management [http://www6.montgomerycountymd.gov/content/council/pdf/res/20081209\\_16-809.pdf](http://www6.montgomerycountymd.gov/content/council/pdf/res/20081209_16-809.pdf)

### **Washington Metropolitan Transit Authority**

- Maryland State Highway Administration <http://sha.md.gov/Home.aspx>
- Maryland Transit Administration -Purple Line: <http://purplelinemd.com/>  
Purple Line Station Neighborhood Work Groups information:  
<http://purplelinemd.com/en/public-involvement/neighborhood-work-groups>
- Maryland's Stormwater Management Act of 2007  
<http://www.mde.state.md.us/programs/Water/StormwaterManagementProgram/Pages/prgorams/waterprograms/sedimentandstormwater/swm2007.aspx>

### **National**

- Americans with Disabilities Act  
[http://www.ada.gov/2010ADASTandards\\_index.htm](http://www.ada.gov/2010ADASTandards_index.htm)





Design  
Guidelines



January 2014

# Chevy Chase Lake Sector Plan



Montgomery County Planning Department  
M-NCPPC  
[MontgomeryPlanning.org](http://MontgomeryPlanning.org)



0114000001